

Guide for Uniform Laws and Regulations Governing Truck Size and Weight Among the WASHTO States

Adopted by the WASHTO Policy Committee
January 2000

Western Association of State Highway and Transportation Officials



WASHTO COMMITTEE ON HIGHWAY TRANSPORT

Chairman: Barry Diseth
Vice Chairman: Regina Phipps
Sponsor: Mary Peters/AZ

State	Primary Delegate	Alternate
ALASKA	Aves Thompson	Peter Shook
ARIZONA	Steve Abney	
CALIFORNIA	Asif Haq	
COLORADO		
HAWAII	Charles Yonamine	
IDAHO	Regina Phipps	Alan Frew
MONTANA	David Galt	Carolyn Knuckles
NEVADA	Jan Christopherson	
NEW MEXICO	Harold Gomez	
NORTH DAKOTA	Gary Berreth	Leanna Wald
OKLAHOMA	Norm Hill	Ron Knox
OREGON	Steve Johnston	
SOUTH DAKOTA	Michael Young	Myron Rau
TEXAS	Lawrence Smith	
UTAH	Rick Clasby	
WASHINGTON	Barry Diseth	Linea Laird
WYOMING	Gary Marsten	Jerry Ellerman
CONTRIBUTORS		
CANADA		
ALBERTA	Vern Janz	
BRITISH COLUMBIA	Samuel Lam	
MANITOBA	Greg Catteeuw	
SASKATCHEWAN	Greg Gilks	
YUKON	John Warkentin	
NORTHWEST TERRITORIES		
MEXICO		
DIRECTOR GENERAL DE TRANSPORTE TERRESTRE		
Ing. Jose Aguilar Alcerreca		
DIRECTOR DE DESARROLLO DEL AUTO TRANSPORTE FEDERAL		
Lic. Dario Arrieta Leyva		

TABLE OF CONTENTS

FOREWORD	7
MISSION	9

CHAPTER 1.00

DEFINITIONS	11
-------------------	----

CHAPTER 2.00

VEHICLES IN REGULAR OPERATION	15
2.01 SCOPE	15
2.02 WIDTH	15
2.03 HEIGHT	15
2.04 LENGTH	15
2.05 MAXIMUM WEIGHT	15
DIAGRAM FOR BRIDGE CHART	17

CHAPTER 3.00

ROUTINELY PERMITTED EXTRA-LEGAL VEHICLES	19
3.01 SCOPE	19
3.02 MAXIMUM WEIGHTS	19
3.03 MAXIMUM DIMENSIONS	19
3.04 TIME OF MOVEMENT	19
3.05 ROUTE RESTRICTIONS	19
3.06 ESCORTS	19
3.07 FLAGS, SIGNS AND WARNING LIGHTS	19
3.08 REGIONAL PERMIT	19
3.09 STATE OF ISSUANCE	20
3.10 ENVELOPE VEHICLE	20
3.11 SPECIAL CONDITIONS	20
3.12 FEES	20

CHAPTER 4.00

MANUFACTURED HOUSING	21
4.01 SCOPE	21
4.02 WIDTH	21
4.03 LENGTH	21
4.04 HEIGHT	21
4.05 TOW VEHICLE REQUIREMENTS	21
4.06 MOVEMENT OF MANUFACTURED HOMES EXCEEDING 14 FT AT THE BASE	21
4.07 TIME OF MOVEMENT	22
4.08 LIGHTS, SIGNS AND ESCORTS	22

CHAPTER 5.00

WARNING DEVICES, ESCORTS AND TIME OF MOVEMENT	23
5.01 SCOPE	23
5.02 LIGHTS, FLAGS AND SIGNS.....	23
5.03 ESCORTS	24
5.04 CONDITIONS REQUIRING ESCORTS	26
5.05 TIME OF MOVEMENT	26
DIAGRAM FOR LIGHTING REQUIREMENTS	28
DIAGRAM FOR FLAGGING REQUIREMENTS	29

CHAPTER 6.00

LONGER COMBINATION VEHICLES	31
6.01 SCOPE	31
6.02 ROCKY MOUNTAIN DOUBLES	31
6.03 TURNPIKE DOUBLES	31
6.04 TRIPLE TRAILERS	31
6.05 MAXIMUM WEIGHTS	32
6.06 PERMITS	32
6.07 LCV GENERAL PERMIT PROVISIONS	32
6.08 LCV EQUIPMENT	32
6.09 LCV OPERATIONAL REQUIREMENTS	33
6.10 LCV DRIVER REQUIREMENTS	33
LONGER COMBINATION VEHICLE FIGURES	35
ROCKY MOUNTAIN DOUBLE	
TURNPIKE DOUBLE	
TRIPLE TRAILER	

CHAPTER 7.00

PERMITS	37
7.01 SCOPE	37
7.02 SINGLE TRIP PERMITS	37
7.03 PERMIT FORMS	37
7.04 ROUTE IDENTIFICATION	37
7.05 METHODS OF SECURING PERMITS	37
7.06 METHODS OF PAYMENT	37
WASHTO PERMIT FORM	39

CHAPTER 8.00

COMMERCIAL VEHICLE ACCIDENT INFORMATION	41
8.01 SCOPE	41
8.02 ACCIDENT REPORTS	41
8.03 VEHICLE CLASSIFICATION	41

CHAPTER 9.00

LIFT AXLES	45
9.01 SCOPE	45
9.02 CONTROLS	45
9.03 AXLE RATING	45
9.04 TIRE RATING	45

APPENDIX	47
REGULAR OPERATION: NATIONAL NETWORK(ENGLISH)	48
REGULAR OPERATION: NATIONAL NETWORK(METRIC)	50
REGULAR OPERATION: NON NATIONAL NETWORK(ENGLISH)	52
REGULAR OPERATION: NON NATIONAL NETWORK(METRIC)	54
NONDIVISIBLE LOAD PERMITS(ENGLISH)	56
NONDIVISIBLE LOAD PERMITS(METRIC)	58
MANUFACTURED HOUSING(ENGLISH)	60
MANUFACTURED HOUSING(METRIC)	62
PILOT/ESCORT VEHICLES(ENGLISH)	64
PILOT/ESCORT VEHICLES(METRIC)	66
LONGER COMBINATION VEHICLES(ENGLISH)	68
LONGER COMBINATION VEHICLES(METRIC)	70
ADDITIONAL LONGER COMBINATION VEHICLES(ENGLISH)	72
ADDITIONAL LONGER COMBINATION VEHICLES(METRIC)	74
VEHICLE WEIGHT LIMITS IN CANADA	76
VEHICLE DIMENSION LIMITS IN CANADA	78
VEHICLE WEIGHT LIMITS IN MEXICO	80
VEHICLE DIMENSION LIMITS IN MEXICO	86
SUPER LOADS MAP	96
SUPERLOAD ALERT PROCEDURES	98
SUPERLOAD INFORMATION TRANSMITTAL SHEET	99
PILOT CAR ESCORT VEHICLE REQUIREMENTS	100
WASHTO RESOLUTION 90-1	105
WASHTO RESOLUTION 7-93	106
WASHTO RESOLUTION 7-95	107
WASHTO RESOLUTION 7-97	108
WASHTO RESOLUTION -99	109

FOREWORD

As we become more aware of our position in the world economic picture it becomes evident that this nation's productivity may be our most important economic problem. As such the productivity of our transportation system plays a large role in our economic progress. The commercial users of our roadway system have pointed out that the interstate transportation of goods on the highway system is becoming snarled and sluggish in the face of proliferation of individual state laws and regulations.

The Western Association of State Highway and Transportation Officials (WASHTO) has recognized this problem and has set forth a process to bring vehicle uniformity to the West and to attack unnecessary regulatory roadblocks that stand in the way of increased productivity. In 1987 WASHTO Chief Administrative Officers adopted a resolution committing development of uniformity in truck size and weight as the top priority of its' Subcommittee on Highway Transport, (which became the Committee on Highway Transport in 1995). This resolution set in motion a rather unique effort to attack the problem. Every state and federal department belonging to WASHTO assigned representatives to the Subcommittee. This insured that not only every state had equal and ample opportunity of involvement, but it produced a group of wide spread experts ranging from top level to first line state representatives who work daily with the laws and regulations governing large vehicles. Early commitment was made by the Subcommittee to heavily involve the private sector in the process. As a result, the Western Trucking Association Executive Council, the Western Highway Institute and the Specialized Carriers and Riggers Association joined the efforts.

The committee is broken into focus groups that have membership from various combinations of the states, industry and federal government members. The committee meets various times as a whole, broke out into intensive focus groups and then come back together to review and approve each and every recommendation. This process insures that all recommendations have a two-level review and that each state is aware of the development of all of the recommendations and has the opportunity to be heard as concerns develop. As the focus groups set out on their task, each first develops an inventory of current laws and regulations relating to their assignment.

This publication is unique in that it is a recommendation of mostly minimum rather than maximum standards. As such it is hoped that the industry can begin to develop an assurance that certain sized vehicles will be able to easily move from state to state without erosion of the delicate balance the states and industry have reached in regulation of large vehicles within each of the states.

The committee understands that each state at times will need to make exceptions based on road configurations and local problems. As such, each state maintains the right to develop special exceptions to these recommendations, but generally the minimum standards are expected to apply to the Interstate and Primary routes, and other roads each state determines best fit their needs.

It is proposed to keep this guide updated as significant changes are made and as the Committee on Highway Transport and the WASHTO Policy Committee expands efforts into additional areas deemed appropriate for establishing uniformity.

The first edition of this WASHTO Guide was published in April of 1990. Since the publication of the original Guide the Subcommittee on Highway Transport has continued to consider and deliberate on truck size and weight issues and has developed additional recommendations for the WASHTO Policy Committee's endorsement. The second edition of the Guide, dated June 1993, was approved by the WASHTO Policy Committee, in June 1993. The third edition of the Guide, dated July 1995, was approved by the WASHTO Policy Committee, in July 1995. This fourth edition of the Guide, dated July 1997, was approved by the WASHTO Policy Committee, in July 1997. This fifth edition of the Guide, dated July 1999, was approved by the WASHTO Policy Committee, in ___ 1999.

WASHTO COMMITTEE ON HIGHWAY TRANSPORT

MISSION STATEMENT

The mission of the WASHTO Committee on Highway Transport in partnership with the motor carrier industry is to promote uniform laws, regulations and practices among member jurisdictions and other jurisdictions for the efficient movement of goods and services while ensuring the safety of all highway users and preserving the highway infrastructure.

GOALS

1. To promote uniformity among the WASHTO states & other jurisdictions regarding statutes, regulations and ITS/CVO and their impacts on the transport industry and highway safety.
2. To serve as a forum to review AASHTO'S proposed policies on highway transport issued from a WASHTO perspective.
3. To review and monitor research, proposals and federal and state regulations related to highway transport as they impact WASHTO members and to promote appropriate truck size and weight research where needed to assist in the fulfillment of the mission of the committee.
4. To provide a forum to share industry best practices among the WASHTO states. To maximize ITS/CVO applications for the safe & efficient movement of goods & services.
5. To provide communication with the motor carrier industry, chief DOT executive offices, other WASHTO committees and other organizations regarding the work and the recommendations of this committee.

CHAPTER 1.00

DEFINITIONS

Words and phrases defined. In this chapter, the following words and phrases have the meaning specified:

1.01 AASHTO GUIDE: A publication entitled Guide For Maximum Dimensions and Weights of Motor Vehicles and for the Operation of Nondivisible Load Oversize and Overweight Vehicles, published by the American Association of State Highway and Transportation Officials.

1.02 Axle: The common axis of rotation of one or more wheels whether power-driven or freely rotating, and whether in one or more segments, and regardless of the number of wheels carried thereon.

1.03 Bus: A motor vehicle designated primarily for the transportation of persons rather than property and having a passenger-carrying capacity of more than 15 persons including the driver, other than a taxi cab constructed and designed for transporting persons for commercial purposes.

1.04 Combination Length: The total length of a combination of vehicles, i.e. truck tractor-semitrailer-trailer combination, measured from front bumper of the motor vehicle to the rear extremity of the last trailer including the connecting tongue(s).

1.05 Combined Trailer Length: The total length of a combination of trailers measured from the front of the first trailer to the rear extremity of the last trailer including the connecting tongue(s).

1.06 Extra-Legal Vehicle: A motor vehicle, laden or unladen which exceeds legal dimensions and/or weights and operates on highways by permit.

1.07 Gross Weight: The weight of a vehicle and/or combination of vehicles plus the weight of any load thereon.

1.08 Height: The total vertical dimension of a vehicle above the ground surface including any load.

1.09 Length: The total longitudinal dimension of a single vehicle, a trailer, or a semitrailer. Length of a trailer or semitrailer is measured from the front of the cargo-carrying unit to its rear, exclusive of all overhang, safety or energy efficiency devices, including air conditioning units, air compressors, flexible fender extensions, splash and spray suppressant devices, bolsters, mechanical fastening devices, and hydraulic lift gates.

1.10 Longer Combination Vehicle: A combination of truck tractor, semitrailer and trailer(s) which exceeds legal length dimensions and operates on highways by permit for transporting reducible loads.

1.11 Manufactured Housing: A structure constructed according to HUD/FHA mobile home construction and safety standards transportable in one or more sections, which is built on a permanent chassis.

1.12 Modular Buildings: A facility designed and constructed (of which does not include a permanent chassis) and is built to state building specifications.

1.13 Maximum Off-Track: The maximum difference in the path created by the center of the steering axle and the center of the rear most axle of the vehicle or vehicle combination during the negotiation of a turn.

1.14 Motor Vehicle: A vehicle which is self propelled or propelled by electric power obtained from overhead trolley wires, but not operating upon rails.

1.15 National Network: Those interstate and other federal-aid primary highways on which commercial vehicles of the dimensions authorized by the Surface Transportation Assistance Act of 1982 are allowed to operate.

1.16 Non Divisible Load: A load which cannot be readily or reasonably dismantled and which is reduced to a minimum practical size and weight. Portions of a load can be detached and reloaded on the same hauling unit provided that the separate pieces are necessary to the operation of the machine or equipment which is being hauled, if the arrangement does not exceed permissible limits.

Federal definition of non divisible load to be used for vehicles operating on the interstate is as follows: any load or vehicle exceeding applicable length or weight limits which, if separated into smaller loads or vehicles would: compromise the intended use of the vehicle, destroy the value of the load or vehicle, require more than 8 work hours to dismantle using appropriate equipment.

1.17 Overhang: Rear overhang, the distance from the center of the last axle to the end of the load, or portion of the vehicle which ever is longer.

1.18 Permit: A written authorization to move or operate on a highway a vehicle or combination of vehicles with or without a load of size and/or weight exceeding the limits prescribed for vehicles in regular operation.

1.19 Pilot/Escort Vehicle: A motor vehicle used for the express purpose as a warning vehicle for extra-legal vehicles.

1.20 Pounds per Inch of Tire Width: A measure of load restriction based on rated tire size. The pounds per inch of tire width are determined by dividing the weight carried by an axle group in pounds by the number of tires in the group and dividing that result by the manufacturers rated tire width as indicated on the sidewall of the tire.

1.21 Regional Permit: Permits issued for movement of certain non-reducible overweight and/or oversize vehicles and/or loads on highways designated by the jurisdictions participating in the "Western Regional Agreement for the Issuance of Permits for Overweight and/or Oversize Vehicles and/or Loads Involved in Interstate Travel".

1.22 Regular operation: The movement over highways of motor vehicles with dimensions and weights specified by state codes.

1.23 Retractable Axle: An axle which can be separately raised and lowered by the driver of the vehicle but which may not have its weight bearing capacity regulated.

1.24 Rocky Mountain Double: A combination of vehicles including a truck tractor pulling a long semi-trailer and a shorter trailer.

1.25 Semitrailer: Every single vehicle without motive power designed for carrying property and so designed in conjunction and used with a motor vehicle that some part of its own weight and that of its own load rests or is carried by another vehicle and having one or more load-carrying axles.

1.26 Single Axle: An assembly of two or more wheels whose centers are in one transverse vertical plane or may be included between two parallel transverse planes 40 inches apart extending across the full width of the vehicle.

1.27 Single Unit: A motor vehicle with no attached vehicles, i.e. truck, bus, truck tractor.

- 1.28 Steering Axle:** The axle or axles on the front of a motor vehicle that are activated by the operator to directly accomplish guidance or steering of the motor vehicle and/or combination of vehicles.
- 1.29 Superload:** A superload is any load that would require special approval by one or more state permit offices because of dimensions or weight. The guidelines contained in this manual reflect maximum size and weight limits for routine issuance of permits. Loads in excess of the stated guidelines would require additional coordination between Industry representatives and State officials. Additionally, loads that do not exceed the superload guidelines may also require special approval by individual states. Refer to page 55 for the superload map and special conditions.
- 1.30 Tandem Axle:** Any two axles whose centers are more than 40 inches but not more than 96 inches apart and are individually attached to or articulated from, or both, a common attachment to the vehicle including a connecting mechanism designed to equalize the load between axles.
- 1.31 Trailer:** Every single vehicle without motive power designed for carrying property wholly on its own structure, drawn by a motor vehicle which carries no part of the weight and load of the trailer on its own wheels and having two or more load carrying axles.
- 1.32 Tow Vehicle:** A motor vehicle used to transport manufactured housing.
- 1.33 Tridem Axle:** Any three consecutive axles whose extreme centers are not more than one hundred forty-four inches apart and are individually attached to or articulated from (or both) a common attachment to the vehicle including a connecting mechanism designed to equalize the load between axles.
- 1.34 Triple Trailer Combination:** A combination of vehicles including a truck tractor pulling a semitrailer and two trailers.
- 1.35 Truck:** A single unit motor vehicle used primarily for the transportation of property.
- 1.36 Truck Tractor:** A motor vehicle used primarily for drawing other vehicles and not so constructed as to carry a load other than a part of the weight of the vehicle and load so drawn.
- 1.37 Trunnion Axle:** An axle configuration with two individual axles mounted in the same transverse plane, with four tires on each axle, connected at a pivot point which allows each individual axle to oscillate in a vertical plane to provide constant and equal weight distribution on each individual axle.
- 1.38 Trunnion Axle Group:** Two or more consecutive trunnion axles, which are individually attached to, and/or articulated, from the vehicle by a weight equalizing suspension system.
- 1.39 Turnpike Double:** A combination of vehicles including a truck tractor pulling a long semitrailer and an additional long trailer.
- 1.40 Variable Load Suspension (VLS) Axle:** An axle which can be separately raised and lowered by the driver of the vehicle. These axles are controlled by hydraulic or air suspension systems or by a combination of these methods.
- 1.41 Vehicle:** A device in, upon, or by which any person or property may be transported or drawn upon a highway, except devices moved by human power or used exclusively upon stationary rails or tracks.
- 1.42 Wide Base Tires:** Wide base single tires are tires whose nominal section width is over 14 inches.
- 1.43 Width:** The total outside transverse dimension of a vehicle including any load or load-holding devices thereon, but excluding approved safety devices and tire bulge due to load.

CHAPTER 2.00

VEHICLES IN REGULAR OPERATION

2.01 SCOPE

The provisions of this chapter governing width, height, length and permissible loads should apply to vehicles serving in regular operation, as defined in Chapter 1.00. Vehicles operating under permit are covered in Chapters 3.00, 4.00, 5.00 and 6.00.

2.02 WIDTH

Maximum vehicle width: 102 inches.

2.03 HEIGHT

Maximum vehicle height: 14 feet.

2.04 LENGTH

2.04.1

Single unit maximum length: 45 feet.

2.04.2

Truck-trailer combination maximum length: 70 feet.

2.04.3

Truck tractor-semitrailer combination maximum length: 65 feet when operated on highways not on the National Network.

2.04.4

Semitrailer maximum length: at least 48 feet on the National Network. Grandfather lengths greater than 48 feet may apply on the National Network.

2.04.5

Combination length for a truck tractor-semitrailer-trailer combination: No maximum on National Network. Combined trailer length for a truck tractor-semitrailer-trailer combination: 61 feet. These provisions allow adequately for double 28 foot or double 28 1/2-foot trailers.

2.05 MAXIMUM WEIGHTS

2.05.1

The maximum load per tire measured by pounds per inch of tire width shall be:

Steering Axle	600 lb/in
All other Axles	500 lb/in

Except for the steering axles or wide base single tires described below, all axles weighing more than 10,000 pounds shall have at least four tires per axle.

In lieu of four tires per axle, an axle may be equipped with wide base tires, limited to 500 pounds per inch of tire width.

Every state reserves the right to permit or otherwise establish limits in excess of those described above for use on vehicle configurations designed to address specific safety or economic concerns.

Each state shall determine effective dates based upon local economic, safety and technological considerations. Amortization of investment and phasing out of single tires to minimize economic and operating disruption of individual companies and effected industries shall be considered factors.

2.05.2

Axle and Gross Weights: The maximum single axle weight shall be 20,000 pounds. The maximum tandem axle weight shall be 34,000 pounds. The maximum gross weight of a vehicle or combination of vehicles computed in accordance with 2.05.3, shall be 80,000 pounds.

2.05.3

Axle Group Weights: The total gross weight in pounds imposed on the highway by any group of two or more consecutive axles on a vehicle or combination of vehicles, shall not exceed the values computed by federal bridge formula "B" as follows:

$$W = 500 ((LN/N-L)+12N+36)$$

Where: W = Maximum weight in pounds carried on any group of two or more axles computed to the nearest 500 pounds.

L = Distance in feet between the extremities of any group of two or more consecutive axles.

N = Number of axles in the group under consideration.

Exceptions to the axle group weights specified above which are federally mandated should be provided for by individual state code.

Bridge Formula Chart

DISTANCE IN FEET BETWEEN FIRST AND LAST AXLE OF ANY GROUP OF CONSECUTIVE AXLES

	2 AXLES	3 AXLES	4 AXLES	5 AXLES	6 AXLES	7 AXLES	8 AXLES	9AXLES
4	34,000							
5	34,000							
6	34,000							
7	34,000							
8	34,000							
8+	38,000	42,000						
9	39,000	42,500						
10	40,000	43,500						
11		44,000						
12		45,000	50,000					
13		45,500	50,500					
14		46,500	51,500					
15		47,000	52,000					
16		48,000	52,500	58,000				
17		48,500	53,500	58,500				
18		49,500	54,000	59,000				
19		50,000	54,500	60,000				
20		51,000	55,500	60,500	66,000			
21		51,500	56,000	61,000	66,500			
22		52,500	56,500	61,500	67,000			
23		53,000	57,500	62,500	68,000			
24		54,000	58,000	63,000	68,500	74,000		
25		54,500	58,500	63,500	69,000	74,500		
26		55,500	59,500	64,000	69,500	75,000		
27		56,000	60,000	65,000	70,000	75,500		
28		57,000	60,500	65,500	71,000	76,500	82,000	
29		57,500	61,500	66,000	71,500	77,000	82,500	
30		58,500	62,000	66,500	72,000	77,500	83,000	
31		59,000	62,500	67,500	72,500	78,000	83,500	
32		60,000	63,500	68,000	73,000	78,500	84,500	90,000
33			64,000	68,500	74,000	79,000	85,500	90,500
34			64,500	69,000	74,500	80,000	85,500	91,000
35			65,500	70,000	75,000	80,500	86,000	91,500
36			68,000	70,500	75,500	81,000	86,500	92,000
37			68,000	71,000	76,000	81,500	87,000	93,000
38			68,000	71,500	77,000	82,000	87,500	93,500
39			68,000	72,500	77,500	82,500	88,500	94,000
40			68,500	73,000	78,000	83,500	89,000	94,500
41			69,500	73,500	78,500	84,000	89,500	95,000
42			70,000	74,000	79,000	84,500	90,000	95,500
43			70,500	75,000	80,000	85,000	90,500	96,000
44			71,500	75,500	80,500	85,500	91,000	96,500
45			72,000	76,000	81,000	86,000	91,500	97,500
46			72,500	76,500	81,500	87,000	92,500	98,000
47			73,500	77,500	82,000	87,500	93,000	98,500
48			74,000	78,000	83,000	88,000	93,500	99,000
49			74,500	78,500	83,500	88,500	94,000	99,500

	2 AXLES	3 AXLES	4 AXLES	5 AXLES	6 AXLES	7 AXLES	8 AXLES	9AXLES
50			75,500	79,000	84,000	89,000	94,500	100,000
51			76,000	80,000	84,500	89,500	95,000	100,500
52			76,500	80,500	85,000	90,500	95,500	101,000
53			77,500	81,000	86,000	91,000	96,500	102,000
54			78,000	81,500	86,500	91,500	97,000	102,500
55			78,500	82,500	87,000	92,000	97,500	103,000
56			79,500	83,000	87,500	92,500	98,000	103,500
57			80,000	83,500	88,000	93,000	98,500	104,000
58				84,000	89,000	94,000	99,000	104,500
59				85,000	89,500	94,500	99,500	105,000
60				85,500	90,000	95,000	100,500	105,500
61				86,000	90,500	95,500	101,000	105,500
62				87,000	91,000	96,000	101,500	105,500
63				87,500	92,000	96,500	102,000	105,500
64				88,000	92,500	97,500	102,500	105,500
65				88,500	93,000	98,000	103,000	105,500
66				89,500	93,500	98,500	103,500	105,500
67				90,000	94,000	99,000	104,500	105,500
68				90,500	95,000	99,500	105,000	105,500
69				91,000	95,500	100,00	105,500	105,500
70				92,000	96,000	101,00	105,500	105,500
71				92,500	96,500	101,50	105,500	105,500
72				93,000	97,000	102,00	105,500	105,500
73				93,500	98,000	102,50	105,500	105,500
74				94,500	98,500	103,00	105,500	105,500
75				95,000	99,000	103,50	105,500	105,500
76				95,500	99,500	104,50	105,500	105,500
77				96,000	100,00	105,00	105,500	105,500
78				97,000	101,00	105,50	105,500	105,500
79				97,500	101,50	105,50	101,500	105,500
80				98,000	102,00	105,50	105,500	105,500
81				98,500	102,50	105,50	105,500	105,500
82				99,000	103,00	105,50	105,500	105,500
83				100,000	104,00	105,50	105,500	105,500
84					104,50	105,50	105,500	105,500
85					105,00	105,50	105,500	105,500
86+					105,50	105,50	105,500	105,500

Some states maximum gross weight limits may be less than or greater than 105,500 pounds.
See appendix in the back of the book for each states maximum gross weight limits with and without permits.

CHAPTER 3.00

ROUTINELY PERMITTED EXTRA-LEGAL VEHICLES - OVERSIZE & OVERWEIGHT

3.01 SCOPE

These guides pertain to routinely permitted nondivisible load vehicles moving over the National Network and other state highways designated by the individual states. It is intended that the guidelines provided herein will establish a basis for WASHTO interstate permit agreements.

3.02 MAXIMUM WEIGHTS

3.02.1

Pounds per inch of tire width: The maximum load on any tire on a vehicle or vehicle combination shall be 600 lb/in. This limitation shall be the only weight control on the steering axle.

3.02.2

Single Axle: The maximum load on a single axle equipped with dual tires (four tires/axle) shall be 24,000 pounds.

3.02.3

Tandem axles: The maximum load on a tandem axle (four tires/axle) shall be 45,000 pounds.

3.03 MAXIMUM DIMENSIONS

3.03.1

Width: Maximum width of the vehicle or combination of vehicles, including load, shall not exceed 10 feet.

3.03.2

Height: Maximum height of the vehicle or combination of vehicles, including load, shall not exceed 14 feet.

3.03.3

Length: Maximum length of the vehicle or combination of vehicles, including load, shall not exceed 90 feet.

3.04 TIME OF MOVEMENT

Extra-legal vehicles, which meet the requirements of this chapter, shall not be subject to time-of-day, weekend or holiday movement restrictions, except for special circumstances dictated by each state.

3.05 ROUTE RESTRICTIONS

Over dimensional vehicles which meet the requirements of this chapter shall be restricted to highways with four or more traffic lanes and shall be further restricted to use of service facilities and commercial areas immediately adjacent to those highways.

3.06 PILOT/ESCORT VEHICLES

Extra-legal vehicles, which meet the requirements of this chapter, shall not require pilot/escorts except for special circumstances dictated by each state.

3.07 LIGHTING, FLAGGING AND SIGNING

Over-dimensional vehicles shall be equipped with flags, signs and lights in accordance with Chapter 5.00.

3.08 REGIONAL PERMIT

The following pertains to the Western Regional Permit, which covers routinely permitted nondivisible vehicles/loads moving over designated highways specified by each member state.

3.09 STATE OF ISSUANCE

A regional permit may be issued by the entry, origin, destination or pass-through state.

3.10 ENVELOPE VEHICLE

The vehicles operating on a regional oversize/overweight permit must comply with the following requirements and maximum dimensions:

3.10.1 Weight

600 lbs. per inch width of tire

21,500 lbs. per single axle (4 tires per axle)

43,000 lbs. per tandem axle (4 tires per axle)

53,000 lbs. per tridem axle (wheelbase more than 8 ft but not more than 13 ft) (4 tires per axle)

160,000 lbs. gross weight

In no case may the gross weight exceed the sum of the permitted axle, tandem axle, group axle weights or the weight specified by the permit, whichever is less.

A minimum of 5 axles is required for the issuance of a regional overweight permit.

3.10.2 Length

110 ft overall

3.10.3 Width

14 ft

3.10.4 Height

14 ft

The oversize load must be nonreducible and can not be a manufactured home.

3.11 SPECIAL CONDITIONS

The member states of Arizona, Colorado, Idaho, Montana, New Mexico, Oklahoma, Oregon, Texas, Utah and Washington have developed a list of special conditions within each state which apply to the operation of the envelope vehicle. This list must accompany all regional overweight/oversize permits.

3.12 FEES

The state of issuance will collect the fees for all member states in which the vehicle is permitted for travel. These fees must be paid by credit card, cash, cashier's check or other guaranteed funds.

CHAPTER 4.00

MANUFACTURED HOUSING

4.01 SCOPE

The provisions of this chapter apply to allowed dimensions of manufactured housing and to minimum requirements for tow vehicles transporting those loads.

It is intended that the guidelines provided herein will establish a basis for WASHTO interstate permit agreements.

4.02 WIDTH

Maximum width of manufactured housing 14 feet plus 2 feet allowed for eaves.

4.03 LENGTH

4.03.1

Maximum length of manufactured housing, including tongue 80 feet.

4.03.2

Maximum length of manufactured housing and tow vehicle 90 feet.

4.04 HEIGHT

Maximum height of manufactured housing 14 feet or limited by vertical height restrictions as determined by route.

4.05 TOW VEHICLE REQUIREMENTS

4.05.1

The tow vehicle shall be equipped with at least one four-wheel drive axle.

4.05.2

Tow vehicle wheelbase shall be a minimum of 100 inches.

4.05.3

The rated gross vehicle weight (GVW) of the tow vehicle shall be based on the base width of the manufactured housing which it is transporting as follows:

Mfg. House Width	Minimum GVW Rating
10 feet or less	6,000 Pounds
10 feet to 12 feet	8,000 Pounds
12 feet and Greater	9,000 Pounds

4.06 MOVEMENT OF MANUFACTURED HOMES EXCEEDING 14 FT AT THE BASE

Minimum equipment and transport requirements for moving manufactured homes that are greater than 14 ft wide at the base.

4.06.01 Axles:

States may consider establishing a minimum number of axles. The number of axles, in any case, must be adequate to ensure that the capacity rating of the axles is not exceeded.

4.06.02 Tires:

The load on the tires shall not exceed the manufacturer's load capacity (sidewall) rating.

4.06.03 Brakes:

All axles must be equipped with operational brakes.

4.07 TIME OF MOVEMENT

Movement shall be allowed in accordance with the provisions of Chapter 5.00.

4.08 LIGHTS, SIGNS AND ESCORTS

Lights, signs and escort requirements for manufactured housing shall meet the provisions of Chapter 5.00.

CHAPTER 5.00

LIGHTING, FLAGGING, SIGNING, PILOT/ESCORT VEHICLE AND TIME OF MOVEMENT REQUIREMENTS

5.01 SCOPE

The provisions of this chapter are intended to supplement legal requirements for motor vehicles and apply to extra-legal vehicles transporting oversize non-divisible loads, including manufactured housing.

5.02 LIGHTS, FLAGS AND SIGNS

5.02.1 LIGHTING REQUIREMENTS FOR OVERSIZE LOADS TRAVELING AFTER DARK.

Those overdimensional loads traveling during hours of darkness shall be required to display lights to mark the extremities of the vehicle and/or load, and shall be in addition to those clearance lights required on legal size vehicles when traveling at night.

1. Standards For Lights On Oversize Vehicles And/Or Loads.
 - a. Lights are only required on those vehicles traveling after dark. (dark is defined as 1/2 hour after sunset to 1/2 hour before sunrise)
 - b. The lights must be visible from a minimum of 500 feet.
 - c. The lights shall be steady burning.
 - d. The color of the lights shall be as follows:
 1. Lights visible from the front of the vehicle and/or load and the extremities in the middle or near the front of the vehicle and/or load shall be amber.
 2. Lights visible from the back of the vehicle and/or load and the extremities near the back of the vehicle and/or load shall be red.
2. Standards for Lights On Rear Overhang.
 - a. Lights are required when rear overhang exceeds the end of the trailer by 4 feet or more.
 1. If the overhang is 2 feet wide or less only one light is required on the end of the load.
 2. If the overhang is over 2 feet wide, two lights are required on the end of the load to show the maximum width of the overhang.

* See diagrams at end of chapter
3. Standards for Warning Lights on Towing Vehicles.
 - a. Amber flashing lights displayed on a towing vehicle operating at night, shall be mounted above the cab and meet the following:
 1. Rotating or flashing beacon visible from a minimum of 500 feet.

5.02.2 FLAGGING REQUIREMENTS FOR OVERSIZE LOADS.

1. Warning Flags. All overwidth and overlength vehicles and/or loads, excluding extra-length vehicle combinations, shall be marked by warning flags meeting the following:
 - a. Warning flags are required on all overwidth loads and loads with rear overhang of 4 feet or more from the end of the trailer.
 - b. Size - 12 inches by 12 inches
 - c. Color - Solid Red or Fluorescent Orange
 - d. Placement of Flags -
 1. On overwidth loads flags shall be placed at the four corners and/or extremities of the vehicle and/or load.
 - Front - Fastened to each front corner of the vehicle and/or load if it exceeds legal width.
 - Rear - Fastened to each corner of the extremities of the vehicle and/or load at the rearmost part of the vehicle or load if it exceeds legal width.

Side - Fastened to mark any extremity of size if wider than the front or the rear.
(see examples)

2. If rear overhang exceeds the end of the trailer by 4 feet or more, 1 flag is required at the extreme rear, if the width of the projection is 2 feet wide or less. If the projection exceeds 2 feet in width then 2 flags are required at the extreme rear to indicate the maximum width.

* See diagrams at end of chapter

5.02.3 SIGNING REQUIREMENTS FOR OVERSIZE LOADS.

Oversize load signs shall meet the following specifications:

1. Dimensions - 18 inches high by 7 feet wide, letter height 10 inches, letter type Standard Series C, stroke width 1 5/8 inch, black letters on yellow background.
2. Displaying Signs - Oversize load signs shall be displayed on the front of the towing vehicle and the rear of the oversize load.
3. When Signs Are Required - Oversize load signs shall be required on all vehicles and/or loads exceeding legal width. Signs shall not be displayed when the vehicle is empty and of legal dimensions.

5.03 PILOTS/ESCORTS

The following requirements are designed for states seeking to implement a pilot/escort certification program (see appendix); however, other states are encouraged to implement as many of the requirements as appropriate.

5.03.1

Oversize Load Signs for Pilots/Escorts: All pilot/escort vehicles while escorting an oversize vehicle and/or load shall display a sign having the words OVERSIZE LOAD. Such signs shall not be displayed and shall be considered illegal except when the pilot/escort vehicle is actually escorting an oversize load.

The dimensions of the sign shall be 10 inches high by 5 feet wide, type Standard Series B, 8 inch high letters, 1 inch stroke width, black letters on yellow background.

5.03.2

Lighting Requirements for Pilot/Escorts:

1. Flashing or Rotating amber lights displayed on pilot/escort vehicles shall be mounted at each end of the required OVERSIZE LOAD sign above the roofline of the vehicle. These lights shall meet the minimum standards outlined under oversize load lighting requirements.
2. As an alternate, a pilot/escort vehicle may display 1 rotating or flashing amber beacon visible from a minimum of 500 feet mounted above the roofline.

5.03.3

The operator of a pilot/escort vehicle must obtain and maintain certification as provided in this chapter to be authorized to escort an extra-legal vehicle or load on a state highway. The permittee of the extra-legal vehicle or load shall be responsible to inspect a pilot car escort vehicle operator's certification to ensure that it is valid and current. To obtain a certification, the operator must:

1. Submit an application for certification to the Department.
2. Pass the certification examination.
3. Possess a valid motor vehicle driver's license.
4. Receive a minimum of eight (8) hours of training specific to escort requirements, including principals of defensive driving.
5. Possess a current certificate of insurance or endorsement which indicates that the operator, or the operator's employer, has in full force and effect commercial liability insurance in at least the minimum

amounts of \$100,000 per person and \$300,000 per accident covering legal liability arising out of an act or omission by the pilot car escort operator of the escort duties required. Such insurance must be maintained at all times during the term of the certification.

6. Provide a state Department of Motor Vehicle record of accidents the operator was involved in during the previous five-year period.

5.03.4

The pilot/escort vehicle shall:

1. Be either a passenger car or a two-axle truck;
2. Not exceed a maximum gross vehicle weight rating of 14,000 pounds
3. Be at least 60 inches wide; and
4. Not exceed the legal limits of size and weight.

5.03.5

The pilot/escort operator shall not carry any item(s) or equipment or load in or on the pilot car escort vehicle which:

1. Exceeds the height, length, or width of the pilot/escort vehicle, or overhangs the pilot/escort vehicle, or otherwise impairs its immediate recognition as a safety pilot/escort vehicle by the motoring public; or
2. Obstructs the view of the flashing yellow lights or the signs used by the pilot/escort vehicle; or
3. Causes safety risks; or
4. Otherwise impairs the performance by the operator or the pilot/escort vehicle.

5.03.6

A pilot/escort vehicle shall carry the following items of equipment at all times when escorting an extra-legal vehicle or load:

1. Standard 18 inch STOP & SLOW paddle sign.
2. Three bi-directional emergency reflective triangles.
3. A minimum of one 5 pound B,C, fire extinguisher.
4. A reflectorized orange vest, shirt, or jacket, which must be worn by the operator when working out of the vehicle.
5. A hard hat, which must be worn by the operator when out of the vehicle.
6. Two additional oversize load signs.

5.03.7

Identification signs or placards shall be displayed on the pilot car escort vehicle at all times when escorting an extra-legal vehicle or load. The sign or placard must:

1. Appear on both sides of the pilot car escort vehicle.
2. Be in letters that contrast sharply in color with background on which the letters are placed.
3. Be readily legible, during daylight hours, from a distance of 50 feet while the vehicle is stationary; and
4. Be kept and maintained in a manner that retains the legibility.

5.03.8

In the performance of the duties required, the operator of the pilot/ escort vehicle may direct other traffic to stop, slow, or proceed in situations where such direction is necessary to allow the extra-legal vehicle or load to continue moving safely, except as provided below.

The operator of the pilot/escort vehicle shall signal the extra-legal vehicle or load to stop, and the extra-legal vehicle or load shall stop as far off of the roadway as practicable to allow other traffic to pass the stopped extra-legal vehicle or load in the following situations:

1. When the extra-legal vehicle or load becomes disabled; or
2. When the movement of the extra-legal vehicle or load on a particular section of state highway presents a safety risk or unreasonably interferes with the efficient movement of other traffic, based upon such factors as the widths of the extra-legal vehicle or load and the roadway, volume of other traffic, visibility and limited sight distance, and mountainous terrain; or
3. When driving conditions for the extra-legal vehicle and/or load are hazardous for any other reason, including weather. When the extra-legal vehicle or load has been stopped pursuant to this Subsection, the pilot car escort operator shall then direct other traffic past the extra-legal vehicle or load, as necessary, until such time as the extra-legal vehicle or load can re-enter the roadway and continue moving without presenting a safety risk or unreasonably interfering with efficient movement of other traffic.

When directing traffic in the situations described in this Section, the operator shall:

1. Stand outside the pilot/escort vehicle;
2. Use the paddle signs and other equipment;
3. Comply with the flagging procedures and requirements described in Section 6 E of the Manual on Uniform Traffic Control Devices, Part 6, Published September 3, 1993.

5.03.9

Reciprocity of the pilot car escort certification program, shall be allowed provided individual states meet the minimum requirements set forth herein.

5.04 CONDITIONS REQUIRING ESCORTS

One escort in advance of the extra-legal vehicle is required for the following listed conditions:

On two-lane, two-way roadways when:

- The load exceeds 12 feet in width, but is 14 feet or less in width.
- The load exceeds 105 feet in length.

On four-lane and divided roadways when:

- The load exceeds 14 feet in width.
- The load exceeds 120 feet in length.

On all roadways when:

- The load overhang exceeds 20 feet.

Two pilot/escorts, one in advance and one following the extra-legal vehicle, are required for the following listed condition:

On two-lane, two-way roadways when:

- The load exceeds 14 feet in width, but is 16 feet or less in width.

5.05 TIME OF MOVEMENT

Movement shall be allowed any day during daylight hours (one half-hour before sunrise to one half-hour after sunset) except the day before the holiday, the holiday, and the day after the holiday for those national holidays listed below:

New Years Day

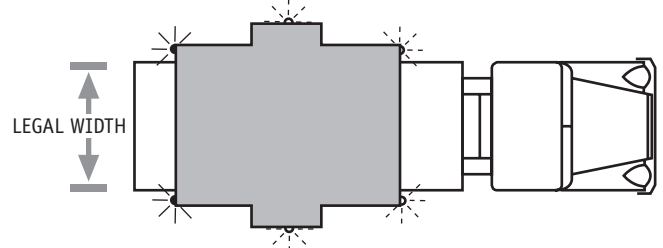
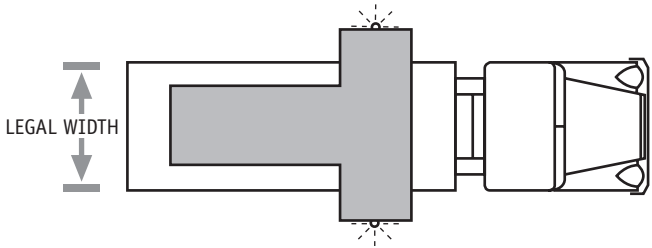
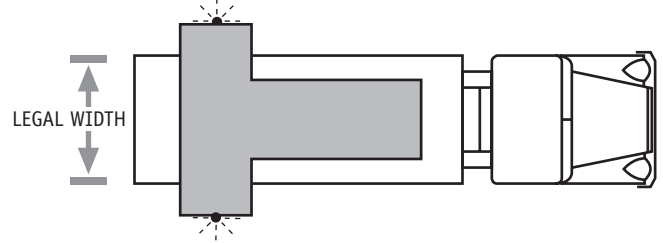
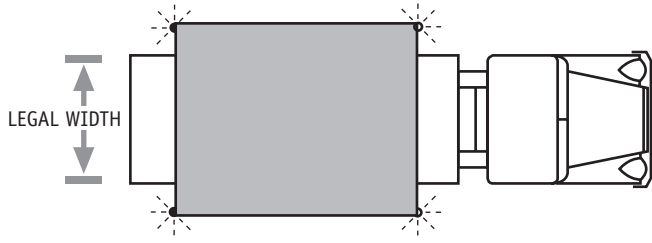
Memorial Day

Independence Day

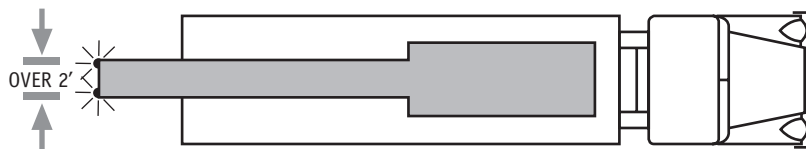
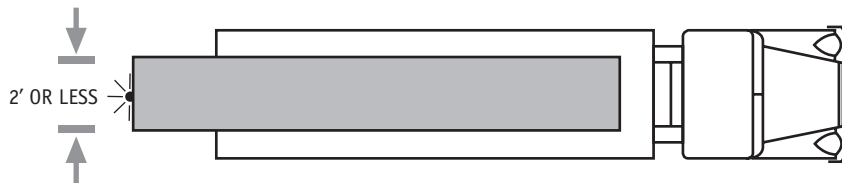
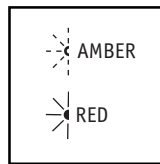
Labor Day

Thanksgiving Day

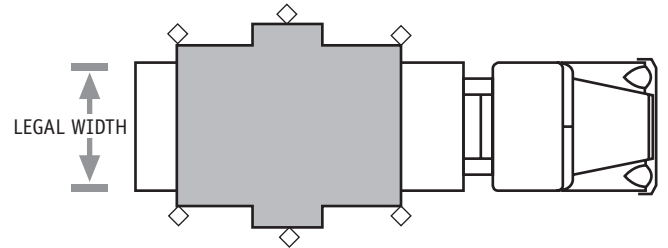
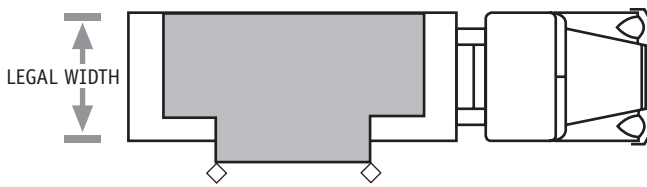
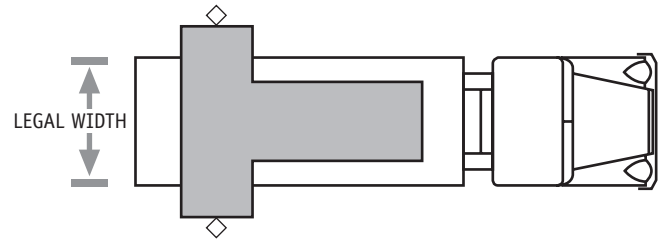
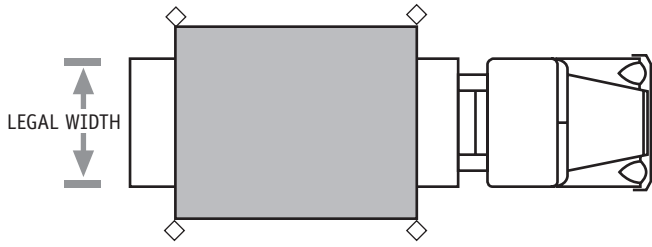
Christmas Day



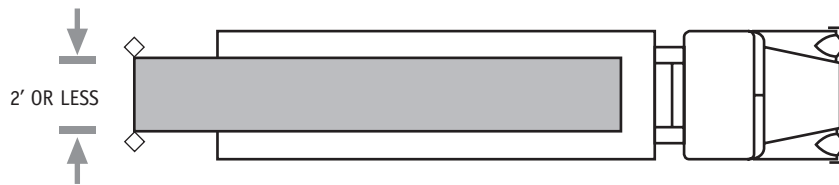
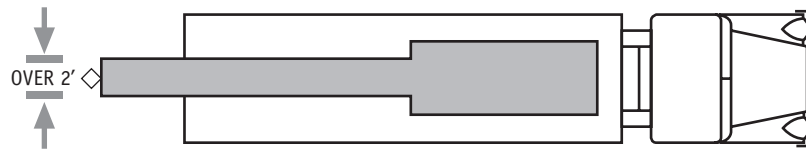
EXAMPLES FOR WARNING LIGHTS ON OVERWIDTH LOADS



EXAMPLES FOR RED WARNING LIGHTS ON OVERLENGTH LOADS WITH A REAR END OVERHANG OF FOUR FEET OR MORE FROM END OF TRAILER



RED OF FLORESCENT ORANGE WARNING FLAGS ON OVERWIDTH LOADS



OR LOADS WITH A REAR END OVERHANG OF FOUR FEET OR MORE FROM END OF TRAILER

CHAPTER 6.00

LONGER COMBINATION VEHICLES (LCVS)

6.01 SCOPE

The provisions of this chapter apply to LCV's. It is intended that each state, which allows LCV's, will designate highway systems for LCV operations.

Further, the provisions of this chapter are intended as a uniformity guide for those WASHTO states, which presently provide for some type of LCV operations. The inclusion of this chapter in the guide should not be interpreted as a WASHTO endorsement of LCV operations for all WASHTO states.

6.02 ROCKY MOUNTAIN DOUBLES

See Figure, Page 35

6.02.1

Combination Length: No Maximum

6.02.2

Combination Trailer Length: 81 Feet Maximum

6.02.3

Trailer Length: 48 Feet Maximum

6.02.4

Optional Dimensions:

Other truck tractor double trailer combinations should be allowed which provide maximum off-track and other operational characteristics similar to or better than a rocky mountain doubles combination including trailers of 48 feet and 28 1/2 feet within 81 feet combination trailer length.

6.03 TURNPIKE DOUBLES

See Figure, Page 35

6.03.1

Combination Length: 110 Feet Maximum

6.03.2

Trailer Combination Length: 95 Feet Maximum

6.03.3

Trailer Length: 45 Feet Maximum

6.03.4

Optional Dimensions:

Other truck tractor double trailer combinations should be allowed which provide maximum off-track and other operational characteristics similar to or better than a turnpike doubles combination with twin 45 feet trailers.

6.04 TRIPLE TRAILERS

See Figure, Page 35

6.04.1

Combination Length: No Maximum

6.04.2

Combination Trailer Length: 95 Feet Maximum

6.04.3

Trailer Length: 28 1/2 Feet Maximum

6.05 MAXIMUM WEIGHTS

Allowable axle group weights and gross vehicle weight of LCV,s should be determined in accordance with Federal Bridge Formula "B" (refer to Chapter 2.00, Section 2.05.2). Maximum gross vehicle weight should be a minimum of 105,500 pounds and a maximum of 129,000 pounds.

6.06 PERMITS

The authority to operate LCV's should be by permit, in accordance with the LCV Model Permit Provisions contained herein:

6.07 LCV GENERAL PERMIT PROVISIONS

1. The driver and company shall be jointly responsible for meeting the permit requirements.
2. No other trailer configurations or multiple trailer combinations needing a permit may be operated under this permit unless these are specifically listed on the permit or otherwise authorized.
3. This permit requires that the permittee have an established safety program. In addition, the permittee must not have an unsafe or unsatisfactory US DOT or other safety rating from any state or province in which he operates. This permit is automatically void if the operator incurs an unsatisfactory rating. The permit will be reinstated when proof has been submitted to the permit authority that the motor carrier has been officially notified by the US DOT, the state or province giving the unsafe or unsatisfactory safety rating that a satisfactory rating has been restored.
4. The permit is valid for those routes designated on the permit. No authority is given to travel on routes other than those listed.
5. This permit may be suspended or revoked for failure to comply with any of the provisions or conditions of this permit. The motor carrier and driver, must comply with all of the motor vehicle laws and the latest motor carrier safety regulations, issued by the US DOT. Traveling faster than the posted speed limit is a permit violation.

6.08 LCV EQUIPMENT

In addition to the requirements of Section 1, the following rules and regulations also apply:

1. All trucks and truck tractors shall be powered to provide adequate acceleration and hill-climbing ability under normal operating conditions to allow speeds compatible with other traffic. The ability to maintain a minimum speed of 20 mph while climbing grades is required unless the state or province highway engineer authorizes exemption for a specific grade.
2. Tires: Each axle must have tires of the same size and construction. Tires must be properly inflated for the load to be carried. In no event shall any tire, wheel or rim exceed the manufacturer's maximum load-carrying limit. A vehicle designed for dual tires may not have the dual tires replaced by a single tire. No single tires on single-axle configurations, except the steering axle, are allowed.
3. All hitch connections must be of a no-slack type, which must be visible and operating. All drawbars, pickup plates and fifth wheels must be rated to exceed the weight carried. Any kingpin must be the solid type and permanently fastened.
4. All braking systems must comply with state, province and federal requirements. In addition, fast air transmission and release valves must be provided for all trailers, semitrailers and converter dollies.
5. Mud flaps or splashguards must be antisail types. Mudguards are required behind each nonsteering axle or set of axles.

6. If a sign is required, it shall read "LONG LOAD" and be at least 7 feet long and 18 inches high. The background shall be yellow with black lettering. Letters shall be at least 10 inches high with a 1.41-inch brush stroke.

Note: If Series E modified letters are used, the brush stroke shall be 2 inches.

6.09 LCV OPERATIONAL REQUIREMENTS

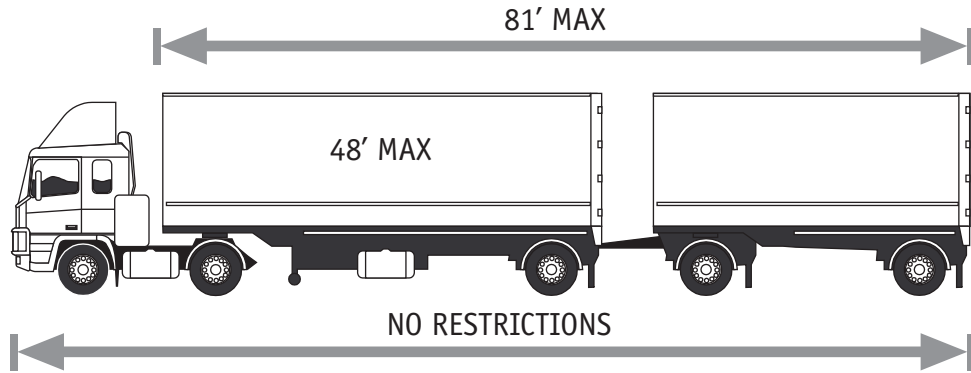
1. Vehicles authorized under this permit must stay in the right lane when practical unless they are in the act of passing.
2. The minimum distance between the permitted vehicle and other trucks shall be at least 100 feet for each 10-mph of speed, except when passing.
3. When the speed of the vehicle drops to 20 mph less than the posted truck limit when climbing grades, the driver must use emergency flashers to notify the passing traffic that they are approaching a vehicle traveling substantially slower than the rest of the traffic.
4. Load Sequence: In no case shall any lighter trailer or semitrailer be placed ahead of a heavier trailer or semitrailer which carries more than 4,000 pounds more than a leading trailer(s). The heaviest trailer or semitrailer shall be placed in front, the lightest in the rear.
5. The operator shall turn on headlights whenever the windshield wipers are in operation or any other time when visibility is poor.
6. Extreme caution in the operation of a motor vehicle shall be exercised when hazardous conditions such as those caused by snow, ice, sleet, fog, mist rain, dust or smoke adversely affect visibility or traction. Speed shall be reduced when such conditions exist. When conditions become sufficiently dangerous, the company or the operator shall discontinue operations, and operations shall not be resumed until the vehicle can be safely operated. The state may restrict or prohibit operations during periods when, in the state's judgment, traffic, weather or other safety conditions make such operations unsafe or inadvisable.

6.10 LCV DRIVER REQUIREMENTS

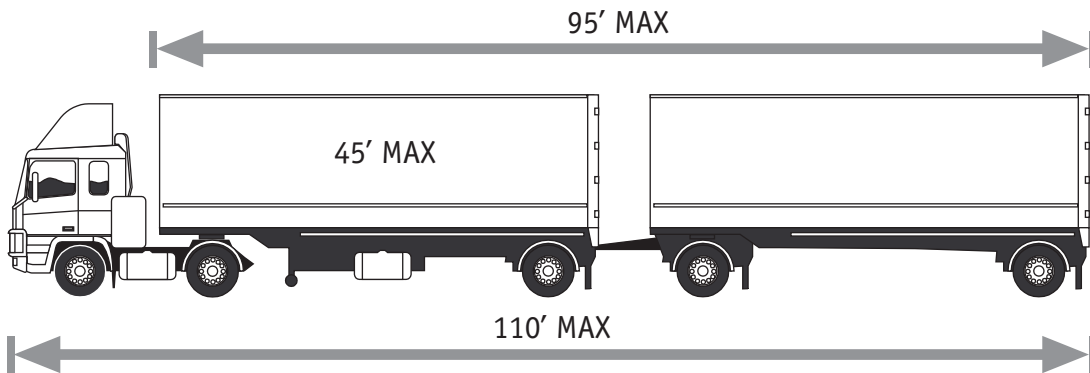
1. All LCV drivers must have a current Commercial Driver's License with the following endorsements: doubles/triples and air brake equipment.
2. LCV drivers and all other individuals or companies operating LCVs must comply with Part 391, Qualifications of Drivers of the Federal Motor Carrier Safety Regulations (FMCSR). Driver exemptions as set forth in sections 391.21 and 391.67 of the FMCSR shall not apply to LCV drivers.
3. All LCV drivers must have a minimum of two years' "linehaul" driving experience driving truck combinations. LCV triple drivers must have a minimum of two years' "line-haul" driving experience driving double-trailer combinations.
4. An LCV driver must be a regularly employed driver and supervised by the company or individual holding the LCV permit.
5. An LCV driver must have had no suspension or revocation of driving privileges in any state or province during the past 24 months where such suspension arose out of operations of a commercial motor vehicle.

LONGER COMBINATION VEHICLES

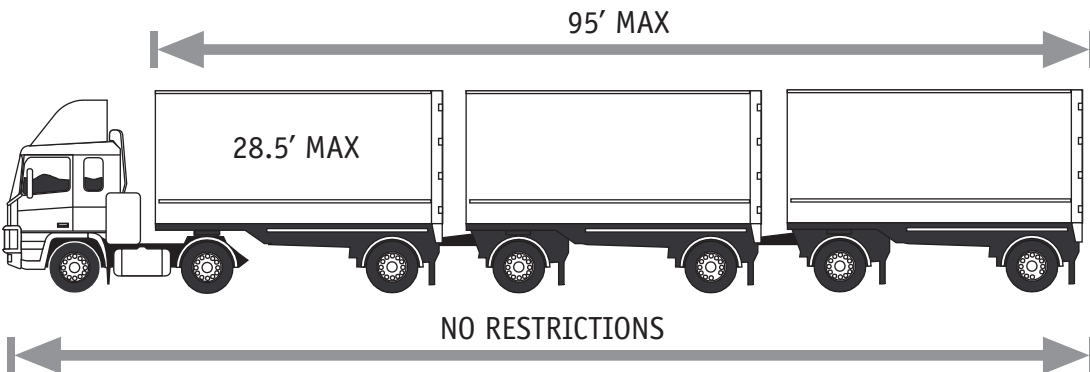
ROCKY MOUNTAIN DOUBLE



TURNPIKE DOUBLE



TRIPLE TRAILERS



CHAPTER 7.00

7.01 SCOPE

It is the intent of this chapter to set forth some standards for permit programs, permit form designs and methods of securing and paying for permits.

Adoption of these standards by the various WASHTO states should accomplish two objectives. The first to provide greater uniformity in the permit process which would aid industry and the second to provide a basis for eventual universal interstate permit agreements between the WASHTO states which would aid both industry and state administrations.

7.02 SINGLE TRIP PERMITS

All WASHTO states should provide for single trip permits.

7.03 PERMIT FORMS

The WASHTO permit form (see figure page 39) should be used as a guide by individual states in developing new permit forms.

7.04 ROUTE IDENTIFICATION

Routes shall be specified on each single trip permit.

7.05 METHODS OF SECURING PERMITS

States should accept permit applications in person, by telephone, by wire, fax and mail.

7.06 METHODS OF PAYMENT

States should accept credit cards for payment of permit fees.

(STATE) APPLICATION / PERMIT FOR OVERSIZE / OVERWEIGHT NON-REDUCIBLE LOAD MOVEMENT

COMPANY NAME
AND ADDRESS

ACCT. NO. _____
COLLECT _____

FAX NO.
PERMIT VALID FROM
ORIGIN
ORIGIN
ROUTE

VIA
TO
DESTINATION
DESTINATION

DESCRIPTION OF LOAD OR VEHICLE

VEHICLE TYPE: TRUCK TRUCK TRACTOR SELF-PROPELLED TRAILER OTHER
 LICENSE NO. _____
 UNIT NO. SERIAL NO. YEAR MAKE

MILES

BASE STATE
P.U.C. NO.

AXLES
AXLES GRP WT
NO. OF TIRES
AXLE SPACING (FT.-IN.)
AXLE SPACING (FT.-IN.)
TIRE SIZE

DIMENSIONS:	OVERALL WIDTH	BASE WIDTH	WIDTH RED RT	WIDTH BLK & INT RT
NO. AXLES	HEIGHT	OVERALL LENGTH	OVERHANG	OVERHANG
GVW				

SPECIAL REQUIREMENTS () FLAG PROJECTIONS () LIGHTS () SIGNS FOR ESCORTS () 2-WAY RADIO COMM.
 () ESCORTS
 () SUNRISE TO SUNSET
 () OTHER

VALID FOR STATE HIGHWAYS & DESIGNATED COUNTIES, OTHER ROUTES SUBJECT TO LOCAL ROAD AUTHORITY.
 HEIGHT AND SPEED RESTRICTIONS SUBJECT TO SPRING BREAKUP.
 PERMITTEE IS RESPONSIBLE FOR VERTICAL CLEARANCES.

APPLICANT'S ACCEPTANCE OF PERMIT CERTIFIES VEHICLE / LOAD HAS BEEN REDUCED TO PRACTICAL MINIMUM SIZE / WEIGHT AND IS HIS/HER AGREEMENT TO COMPLY WITH ALL APPLICABLE STATE PERMIT REGULATIONS AND ALL APPLICABLE STATE OR FEDERAL MOTOR VEHICLE LAWS.

FEE\$
•• TOTAL

SIGNATURE OF APPLICATION

ISSUED:
TIME
DATE
BY

PERMIT NO.

CHAPTER 8.00

COMMERCIAL VEHICLE ACCIDENT INFORMATION

8.01 Scope

There is a growing need in the United States for comprehensive accident information concerning trucks and truck combinations. It is the intent of this chapter to encourage all WASHTO States to actively work toward an accident records system and a vehicle classification system which will allow the computation of accident rates based on exposure (millions of miles or kilometers traveled) of each specific vehicle or combination type.

That information would be extremely useful for such programs as truck safety enforcement, routing, economic studies and vehicle size/weight standards.

The Federal Highway Administration (FHWA) has for many years required vehicle classifications by 13 generalized categories. That classification system is not considered adequate to provide the type of information now required, especially since it does not differentiate between vehicles in regular operation and more productive vehicle combinations including longer combination vehicles which normally operate by authority of permits. The FHWA vehicle classification system, for instance, does not recognize the difference between a federally mandated double 28' trailer combination and a turnpike doubles combination. The operational characteristics of these two vehicle types is considerably different and accident rates are very difficult to come by due to the FHWA classification system which puts both combinations in the same category.

8.02 Accident Reports

Information concerning commercial vehicle accidents is best obtained from statewide uniform police accident reports. The report form should adequately display the appropriate vehicle types (silhouette likeness are recommended). It is important that police officers be trained to furnish the correct information. Also the report form should provide adequate information for the reporting officer to place the vehicle in the proper classification. See figure on page 43, which portrays a suggested format of vehicle classifications for the police report form. All of the vehicles portrayed in the figure may not be appropriate for all states.

8.03 Vehicle Classification

To obtain accident rates for each type of commercial vehicle requires the vehicle classification system to also have the capability of providing total vehicle miles of travel for each vehicle and combination of vehicles denoted on the accident form. Methods of collecting raw vehicle classification data, which can later be used to format vehicle classification data into the standard FHWA, format or into the format proposed in this Guide are now possible by employing a relatively new concept. The American Society for Testing and Material (ASTM) now has in place a Standard Practice for Classifying Highway Vehicles from Known Axle Count and Spacing-ASTM designation E 1572-93.

LEGAL COMMERCIAL VEHICLES

1) SINGLE UNIT TRUCK



2) TRUCK / TRAILER



3) BOBTAIL TRUCK TRACTOR



4) SADDLE MOUNT COMBINATION



5) TRUCK TRACTOR / SEMITRAILER



6) TRUCK TRACTOR / SEMITRAILER / TRAILER



7) OTHER COMMERCIAL VEHICLE

SPECIALIZED VEHICLES

8) SPECIAL MOBILE EQUIPMENT (CONSTRUCTION, FIRE, ETC.)



9) FARM TRACTOR AND / OR EQUIPMENT



PERMITTED COMMERCIAL VEHICLE COMBINATIONS

10) TRUCK / TRAILER



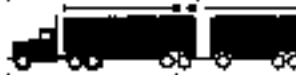
11) TRUCK / 2 TRAILERS



12) TRUCK TRACTOR / SEMITRAILER



13) ROCKY MOUNTAIN DOUBLES



14) TURNPIKE DOUBLES



15) TRUCK TRACTOR / 3 TRAILERS



16) OTHER COMMERCIAL VEHICLE

*STATES MAXIMUM LEGAL LENGTH
** STATES MAXIMUM PERMITTED RANGE OF LENGTHS

CARGO BODY TYPE:

- | | | | | |
|-----------------------|-----------------|---------------------|--------------------------|-----------|
| 1) BUS | 4) CARGO TANK | 7) DUMP BED | 10) BOAT TRANSPORTER | 13) OTHER |
| 2) VAN | 5) FLATBEAD | 8) CONCRETE MIXER | 11) GARBAGE | |
| 3) SHIPPING CONTAINER | 6) BULK FREIGHT | 9) AUTO TRANSPORTER | 12) POLE OR LOG TRAILERS | |

CHAPTER 9.00

LIFT AXLES

9.01 Scope

The purpose of this chapter is intended to promote safety and reduce the pavement damage resulting from operating with retractable and/or variable load suspension (VLS) axles. The following guidelines are intended to apply to divisible loads.

9.02 Controls

The up-down switch may be inside the cab.

The pressure regulator valve shall be positioned outside of the cab and be inaccessible from the driver's compartment.

9.03 Axle Rating

Axles shall not exceed their design load capacity as indicated by an attached data plate or written certification from a vendor.

9.04 Tires

Tires shall not exceed their rated load capacity (sidewall) rating as specified by the manufacturer.

APPENDIX

Current Truck Size and Weight Regulations for Vehicles in Regular Operation: National Network*

	AK	AZ	CA	CO	HI	ID	MT	NV
Height	14'	14'	14'	13'	14'	14'	14'	14'
Width	8' 6"	8' 6"	8' 6"	8' 6"	9'	8' 6"	8' 6"	8' 6"
Single Unit Length	40'	40'	40'	45'		45'	55'	40'
Truck-Trailer								
Overall Length	95'		65'	70'		75'	75'	70'
Tractor-Semitrailer								
Overall Length	NS	NS	Unlimited			NS		
Trailer Length	53'	57' 6"	48'/53'	57' 4"		53'	53'	
Kingpin Distance	41'	NS	Unlimtd/40'			NS		
Tractor-Semitrailer-Trailer (Doubles)								
Overall Length	120'	NS	Unlimited	^A		NS		70' ^A
Combined trailer Length	95'	NS	N/A			68'	61'	
Individual Trailer Length	48'	28' 6"	28' 6"	28' 6"		NS	28' 6"	
Weights/Inch Of Tire Width								
Steering Axle	600#	NS	620			600#	^F	
Other Axles With Single Tires	550#		620			600#	^G	
Other Axles Without Single Tires								
Axle Weights (In 1000 Kg)								
Steering Axle	20	20	20	20		20	20	
Single Axle	20	20	20	20	22.5	20	20	20
Tandem Axle	38	34	34	36	34	34 ^B	34	34
Gross Weight (In 1000 Kg)								
Interstate Routes	Uncapped	80	80	80	80	80 ^C	80	80
Non-Interstate Routes			80			105.5		
Federal Bridge Formula?	Yes	Yes	Yes ^H	Yes	Yes	Yes	Yes	Yes

Footnotes:

NA ...Not Available

NS....Not Specified

ANone unless semitrailer or trailer exceeds 28' 6"

*And designated access routes

B 37,800 for special agricultural and mining products on interstate up to GVW 79,000#

CBy permit on Interstate over 80,000 GVW

DNot to exceed 550 lbs per inch width of tire

ESingle trailers permitted to 56 ft, double trailers to 68 ft

F.....Manufacturers tire rating

G500 lbs on wide base tires only

H With exceptions

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
14'	14'	13' 6"	14'	14'	14'	14'	14'	14'	14'
8' 6"	8' 6"	8' 6"	8' 6"	8' 6"	8' 6"	8' 6"	8' 6"	8' 6"	8' 6"
40'	50'	45'	40'	45'	45'	45'	40'	60'	45'
65'	75'	70'	75'	80'	65'	65'	75'	85'	70'
65'								NS	65'
57' 6"	53'	59' 6"	53'	53'	59'	48'	53' ^E	60'	48'
						41'		NS	
65'								NS	
28' 6"		110'	68'	81' 6"		61'	61' ^E	81'	61'
	53'	53'	40'	45'	28' 6"		53'		
600#	550#		600#	600#	650#	^F	600#	750#	600#
600#	550#		600#	550#	650#	600#	500#	600#	500#
20	20 ^D	20	20	20		20	20	20	
21.6	20	20	20	20	20	20	20	20	20
34.32	34	34	34	34	34	34	34	36	34
86.4	105.5	80	80	Uncapped ^C	80	80	105.5	117	80
								117	
Modified	Yes	Yes	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Current Truck Size and Weight Regulations for Vehicles in Regular Operation: National Network*

	AK	AZ	CA	CO	HI	ID	MT	NV
Height	4.26 m	4.26 m	4.26 m	3.96 m	4.26 m	4.26 m	4.26 m	4.26 m
Width	2.59 m	2.59 m	2.59 m	2.59 m	2.74 m	2.59 m	2.59 m	2.59 m
Single Unit Length	12.19 m	12.19 m	12.19 m	13.71 m		13.71 m	16.76 m	12.19 m
Truck-Trailer								
Overall Length	28.95 m		19.81 m	21.33 m		22.86 m	22.86 m	21.33 m
Tractor-Semitrailer								
Overall Length		NS	Unlimited			NS		
Trailer Length	16.15 m	17.52 m	14.63m/16.1m	17.47 m		16.15 m	16.15 m	
Kingpin Distance	12.49 m	NS	Unlimtd/12.19m			NS		
Tractor-Semitrailer-Trailer (Doubles)								
Overall Length	36.57 m	NS	Unlimited			NS		21.33 m
Combined trailer Length	28.95 m	NS	N/A			20.72 m		
Individual Trailer Length	14.63 m	8.68 m	8.68 m	8.68 m		NS	8.68 m	
Weights/Inch Of Tire Width								
Steering Axle	272 kg	NS	N/A			272 kg	F	
Other Axles With Single Tires	249 kg		N/A			272 kg	G	
Other Axles Without Single Tires								
Axle Weights								
Steering Axle	9,071 kg	9,071 kg	9,071 kg	9,071 kg		9,071 kg	9,071 kg	
Single Axle	9,071 kg	9,071 kg	9,071 kg	9,071 kg	10,205 kg	9,071 kg	9,071 kg	9,071 kg
Tandem Axle	17,236 kg	15,422 kg	15,422 kg	16,329 kg	15,422 kg	15,422 kg ^B	15,422 kg	15,422 kg
Gross Weight								
Interstate Routes	Uncapped	36,287 kg	36,287 kg	36,287 kg	36,287 kg	47,853 kg ^C	36,287 kg	36,287 kg
Non-Interstate Routes			36,287 kg					
Federal Bridge Formula?	Yes	Yes	Yes ^H	Yes	Yes	Yes	Yes	Yes

Footnotes:

NA ...Not Available

NS.... Not Specified

m meters

kg kilograms

ANone unless semitrailer or trailer exceeds 8.68 m

* And designated access routes

B 17,145 kg for special agricultural and mining products on interstate up to GVW 35,833 kg

DNot to exceed 249 kg per inch width of tire

E Single trailers permitted to 17.07 m, double trailers to 20.73 m

F Manufacturers tire rating

G 226 kg on wide base tires only

H With exceptions

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
4.26 m	4.26 m	4.11 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m
2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m
12.19 m	15.24 m	13.71 m	12.19 m	13.71 m	13.71 m	13.71 m	12.19 m	18.28 m	13.71 m
19.81 m	22.86 m	21.33 m	22.86 m	24.38 m	19.81 m	19.81 m	22.86 m	25.90 m	21.33 m
19.81 m								NS	19.81 m
17.52 m	16.15 m	18.13 m	16.15 m	16.15 m	17.98 m	14.63 m	16.15 m ^E	18.28 m	14.63 m
						12.49 m		NS	
19.81 m								NS	
8.68 m		33.52 m	20.72 m	24.84 m		18.59 m	18.59 m ^E	24.68 m	18.59 m
	16.15 m	16.15 m	12.19 m	13.71 m	8.68 m		16.15 m	^H	
272 kg	249 kg		272 kg	272 kg	295 kg	^F	272 kg	340 kg	272 kg
272 kg	249 kg		272 kg	226 kg	294 kg	272 kg	226 kg	272 kg	226 kg
9,071 kg	9,071 kg ^E	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg
9,797 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg
15,567 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	16,329 kg	15,422 kg
39,190 kg	47,853 kg	36,287 kg	36,287 kg	36,287 kg ^C	36,287 kg	36,287 kg	47,853 kg	53,070 kg	36,287 kg
								53,070 kg	
Modified	Yes	Yes	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Current Truck Size and Weight Regulations for Vehicles in Regular Operation: Non National Network

Vehicle & Load Dimensions	AK	AZ	CA	CO	HI	ID	MT	NV
Height	14'	14'	14'	13'	14'	14'	14'	14'
Width	8'6"	8'6"	8'6"	8'6"	9'	8'6"	8'6"	8'6"
Single Unit Length	40'	40'	40'	45'	40'	45'	55'	40'
Truck-Trailer								
Overall Length	75'		65'	70'	65'	75'	75'	70'
Tractor-Semitrailer								
Overall Length	75'	65' ^B	65' ^C	57'4"	60'	65' ^O		70'
Trailer Length	48'	53'	Unlimited	57'4"		48'	53'	
Kingpin Distance	NS	NS	40'			NS		
Tractor-Semitrailer-Trailer								
(Doubles)								
Overall Length	75'	NS	75'	28'6"	65'	75' ^G		70' ^F
Combined trailer Length		NS	N/A			61'	61'	
Individual Trailer Length	48'	28'6"	28'6"	28'6"		NS	28'6"	
Weights/Inch Of Tire Width								
Steering Axle	600#	NS	620			600#		^P
Other Axles With Single Tires	550#		620			600#		^Q
Other Axles Without Single Tires								
Axle Weights (In 1000 Lbs.)								
Steering Axle	20	20	20	20		20	20	
Single Axle	20	20	20	20	22.5	20	20	20
Tandem Axle	38	34	34	40' ^I	34	37.8	34	34
Gross Weight (In 1000 Lbs.)								
Non Interstate Routes	Uncapped	80	80	85	88	105.5	80	80
Federal Bridge Formula?	Yes	Yes ^R	Yes	^I	^J	Yes	Yes	Yes

Footnotes:

- NSNot Specified
- BNone unless the semitrailer exceeds 53'
- CNone unless the semitrailer exceeds 57' 4"
- DSingle trailers permitted to 56 ft, double trailers to 68 ft
- ESome Non National Network highways are 88' and 110'
- FNone unless semitrailer or trailer exceeds 28' 6"
- GNone unless trailer combination length is exceeded
- H48' semitrailer, 40' trailer
- IFormula: $W = 1000 \times (L+40)$
- JFormula: $W = 900 \times (L+40)$ on non-interstate
- OOn designated routes
- PManufacturers tire rating
- Q500 lbs wide base tires only
- RWith exceptions

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
14'	14'	13' 6"	14'	14'	14'	14'	14'	14'	14'
8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"
40'	50'	45'	40'	45'	45'	45'	40'	60'	45'
65'	75'	70'	75'	80'	65'	65'	75'	85'	70'
65' ^D	75' ^E		60'					NS	65'
57' 6"	53'	53'		53'	59'	48'	53'	60'	48'
						41'		NS	
65' ^D	75' ^E		75'					NS	
28' 6"		62'		81'6"		61'	61' ^D	81'	61'
	53'	29'	40'	45'	28'6"	28' 6"	53'	^H	
600#	550#		600#	600#	650#	^P	600#	750#	600#
600#	550#		600#	500#	650#	600#	500#	600#	500#
20	20	20	20	20	20	20	20	20	
21.6	20	20	20	20	20	20	20	20	20
34.32	34	34	34	34	34	34	34	36	34
86.4	105.5	90	80	Uncapped	80	80	105.5	117	80
Modified	Yes	Modified	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Current Truck Size and Weight Regulations for Vehicles In Regular Operation: Non National Network

	AK	AZ	CA	CO	HI	ID	MT	NV
Height	4.26 m	4.26 m	4.26 m	3.96 m	4.26 m	4.26 m	4.26 m	4.26 m
Width	2.59 m	2.59 m	2.59 m	2.59 m	2.74 m	2.59 m	2.59 m	2.59 m
Single Unit Length	12.19 m	12.19 m	12.19 m	13.71 m	12.19 m	13.71 m	16.76 m	12.19 m
Truck-Trailer								
Overall Length	22.86 m		19.81 m	21.33 m	19.81 m	22.86 m	22.86 m	21.33 m
Tractor-Semitrailer								
Overall Length	22.86 m	19.81 m ^B	19.81 m ^C	18.28 m	19.81 m ^O			21.33 m
Trailer Length	14.63 m	16.51 m	Unlimited	17.47 m		14.63 m	16.51 m	
Kingpin Distance	NS	NS	12.19 m			NS		
Tractor-Semitrailer-Trailer (Doubles)								
Overall Length	22.86 m	NS	22.86 m ^F	19.81 m	22.86 m ^G			21.33 m ^F
Combined Trailer Length		NS	N/A			18.59 m		
Individual Trailer Length	14.63 m	8.71 m	8.68 m	8.68 m	8.68 m	NS	8.68 m	
Weights/Inch of Tire Width								
Steering Axle	272 kg	NS	N/A			272 kg		^P
Other Axles With or Without Single Tires	249 kg		N/A			272 kg		^Q
Axle Weights								
Steering Axle	9,071 kg	9,071 kg	9,071 kg	9,071 kg		9,071 kg	9,071 kg	
Single Axle	9,071 kg	9,071 kg	9,071 kg	9,071 kg	10,205 kg	9,071 kg	9,071 kg	9,071 kg
Tandem Axle	17,236 kg	15,422 kg	15,422 kg	18,143 kg ^I	15,422 kg	17,145 kg	15,422 kg	15,422 kg
Gross Weight	Uncapped	36,287 kg	36,287 kg	38,555 kg	39,916 kg	47,853 kg	36,287 kg	36,287 kg
Federal Bridge Formula?	Yes	Yes ^R	Yes	^I	^J	Yes	Yes	Yes

Footnotes:

m meters

kg kilograms

B None unless the semitrailer exceeds 16.15 m

C None unless the semitrailer exceeds 17.47 m

D Single trailers permitted to 17.07 m, doubles to 20.73 m

E Some Non National Network highways are 26.82 m and 33.52

F None unless semitrailer or trailer exceeds 8.68 m

G None unless trailer combination length is exceeded.

H 14.63 m semitrailer, 12.19 m trailer

I Formula: $W = 453 \text{ kg} \times (L+12.19 \text{ m})$

J Formula: $W = 408 \text{ kg} \times (L+12.19 \text{ m})$ on non-interstate

O On designated routes

P Manufacturers tire rating

Q 226 kg wide base tires only

R With exceptions

Current Truck Size and Weight Regulations for Vehicles In Regular Operation: Non National Network Page 2

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
4.26 m	4.26 m	4.11 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m
2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m
12.19 m	15.24 m	13.71 m	12.19 m	13.71 m	13.71 m	13.71 m	12.19 m	18.28 m	13.71 m
19.81 m	22.86 m	21.33 m	22.86 m	24.38 m	19.81 m	19.81 m	22.86 m	25.90 m	21.33 m
19.81 m	22.86 m		18.28 m					NS	19.81 m
17.52 m	16.51 m	16.51 m		15.61 m	17.98 m	14.63 m	16.51 m ^D	18.28 m	14.63 m
						12.49 m		NS	
19.81 m ^D	22.86 m ^E		22.86 m					NS	
8.68 m		18.89 m		24.84 m		18.59 m	18.59 m ^D	24.68 m	18.59 m
	15.61	8.83 m	12.19 m	13.71 m	8.68 m	8.68 m	16.51 m	^H	
272 kg	249 kg		272 kg	272 kg	294 kg	^P	272 kg	340 kg	272 kg
272 kg	249 kg		272 kg	226 kg	294 kg	272 kg	226 kg	272 kg	226 kg
9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	
9,797 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg	9,071 kg
15,567 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	15,422 kg	16,329 kg	15,422 kg
39,190 kg	47,853 kg	40,823 kg	36,287 kg	Uncapped	36,287 kg	36,287 kg	47,853 kg	53,070 kg	36,287 kg
Modified	Yes	Modified	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Current State Regulations for Non-Divisible Load Permits: National Network and Other Designated Highways

	AK	AZ	CA	CO	HI	ID	MT	NV
Maximum Weights								
Steering Axle:								
Lb/in tire width	NS	NS		A		600	A	B
1000's lbs			B		23			
Single Axle (1000 lbs)	NS	28	23/30 ^F	27	23	C	C	24
Tandem (1000 lbs)	NS	46	46/60	50	34	C	C	E
Restrictions Because of:								
Darkness	NS	No	Yes	No	No	Yes	Yes	No
Legal Holidays	NS	No	Yes	Yes	No	Yes	Yes	No
Weekends	NS	No	Yes	No	No	Yes	Yes	No
Inclement Weather	NS	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Escort Vehicles Required:	NS	No ^F	Yes ^I	Yes ^F	No	Yes ^F	Yes	No

Footnotes:

NS..... Not Specified

A Manufacturers rating

B Axle rating to 20,000#; tire size over 20,000#

C Depends on axle spacings and route

D Escort required if bridge restrictions of center line & 5 mph apply

E 40,495 to 46,725

F There are exceptions

G Except under special circumstances

H Only if in combination with adjacent tandem or tridem with common suspension

I If over lane width

J Unless overweight only

K 10 ft wide x 100 ft long allowed on Interstate highways at night

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
600	600	13/15 ^C	600	600	650	^A	600	750	600
26	24	20	21.5	31	25	29,500	22	25	24
46	45	40	43	52	46	50	43	55	45
Yes	Yes ^J	Yes	No	No	Yes ^K	Yes	Yes ^F	No	No
Yes	Yes	Yes	No	No	Yes ^F	Yes	Yes	No	No
Yes	Yes	Yes	No	No	Yes	Yes	No	No	No
Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No
Yes ^F	Yes ^F	No	No	No ^D	No ^F	Yes	Yes ^F	No ^D	No ^G

Current State Regulations for Non-Divisible Load Permits: National Network and Other Designated Highways

	AK	AZ	CA	CO	HI	ID	MT	NV
Maximum Weights								
Steering Axle:								
kg/in tire width	NS	NS		A		272 kg	A	B
kg			B		10,205 kg			
Single Axle	NS	12,700 kg	10,432 kg/ 13,607 kg ^F	12,246 kg	10,205 kg	C	C	10,886 kg
Tandem	NS	20,865 kg	20,865 kg/ 27,215 kg	22,679 kg	15,422 kg	C	C	E
Restrictions Because of:								
Darkness	NS	No	Yes	No	No	Yes	Yes	No
Legal Holidays	NS	No	Yes	Yes	No	Yes	Yes	No
Weekends	NS	No	Yes	No	No	Yes	Yes	No
Inclement Weather	NS	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Escort Vehicles Required:	NS	No ^F	Yes ^I	Yes ^F	No	Yes ^F	Yes	No

Footnotes:

kg kilograms

NS Not Specified

A Manufacturers rating

B Axle rating to 9,071 kg; tire size over 9,071 kg

C Depends on axle spacings and route

D Escort required if bridge restrictions of center line & 8 km per hour apply

E 18,368 kg to 21,194 kg

F There are exceptions

G Except under special circumstances

H Only if in combination with adjacent tandem or tridem with common suspension

I If over lane width

J Unless overweight only

K 10 ft wide x 100 ft long allowed on Interstate highways at night

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
272 kg	272 kg		272 kg	272 kg	294 kg	^A	272 kg		272 kg
		13/15 ^C						11,339 kg	
11,793 kg	10,866 kg	9,071 kg	9,752 kg	14,061 kg	11,339 kg	13,380 kg	9,979 kg	11,339 kg	10,886 kg
23,586 kg	20,411 kg	18,143 kg	19,504 kg	23,586 kg	23,586 kg	22,679 kg	19,504 kg	24,947 kg	20,411 kg
Yes	Yes ^J	Yes	No	No	Yes ^K	Yes	Yes ^F	No	No
Yes	Yes	Yes	No	No	Yes ^F	Yes	Yes	No	No
Yes	Yes	Yes	No	No	Yes	Yes	No	No	No
Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No
Yes ^F	Yes ^F	No	No	No ^D	No ^F	Yes	Yes ^F	No ^D	No ^G

Current State Regulations for Transporting Manufactured Housing: National Network and Other Designated Highways

	AK	AZ	CA	CO	HI	ID	MT	NV
Dimensions								
Width	A	A	16'	A	14'	16'	18'	14'
Length (inc. tongue)	A	A	80'	A		80'		
Length (inc. tow vehicle)	A	A	105'	A	70'	110'		85'
Height	A	A	A	A	A	A	A	A
Tow Vehicle								
Wheelbase	100"	99"		N/A			100"	
Gross Vehicle Weight	10,000	B	C			D	D	F
Axle Requirements	J	J				J	J	J
Movement Restrictions								
Night	Yes	A	Yes	Yes ^O	Yes	Yes	Yes	Yes
Weekend	Yes	A	Yes	Yes ^O	Yes	Yes	Yes	Yes
Holiday	Yes	A	Yes	Yes ^O		Yes	Yes	
Inclement Weather	Yes					Yes	Yes	
Requirements								
Lights	Dot					Solid	Flash	Flash
Lights	Beacon	Dot	Dot	Dot	Dot	Dot	Dot	Dot
Signs	Yes	Yes	Yes	Yes	Yes	Yes	if>10'	Yes
Pilot/Escort	if>10' ^X	Yes	Yes	Yes	if>12'	Yes	if>12'	Yes
Permits	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Route Identification	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

Footnotes:

- A Limited by route.
- B 1.5 ton - 10 x 50; 2 ton - over 10 x 50
- C 6,500# 8-12' wide; 9,500# over 12'
- D 8,000# 10-12' wide; 9,000# over 12'
- E Plus 12" eave
- F 3/4 ton less than 18,000#; 1-1/2 ton over 18,000#
- G 3/4 ton 10' or less wide; 2 ton over 10'
- H 89" cabover 120" conventional
- I 6,000# less than 10'; 8,000# 10-12'; 9,000# over 12'
- J 4 tires on driving axle
- K No movement on Sunday
- L 6,000# less than 10' wide; 8,000# 10-12'; 9,000# over 12'
- M Yes if > 16' on Interstate or if > 20' on other highways
- N Dot and rotating if over 12' wide
- O Or limited by route.
- P 1/2 hour before sunrise to 1/2 after sunset
- Q Restricted on the following holidays as well as the day before and the day after: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day and Christmas.
- R Flashing or rotating beacon on top of tow vehicle, plus flashing or rotating beacon on roof at rear of manufactured home if escort vehicles not required.
- S Signs meeting AASHTO guide required if manufactured home over 102" wide
- T Escorts required when over 12' wide on 2 lane hwy and over 14' wide on multi-lane hwy. Escorts must be at least 60" wide and no larger than 1-3/4 ton pickup
- U Plus 2' eaves.
- V Discretion of permit authority
- W 16' base width allowed if hauled on licensed trailer
- X Or if overall length is greater than 85 feet
- Y 14' or > on 2 lane, 15' or > on divided

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
16'	18'	16' ⁰	14'	A	A	16'	14' ^{A,E,W}	A	14' ^U
85'			75'	A					80'
101'	120'		80'	A	A		75'		90'
A	A	A	A	A	A	A	A	A	14' ⁰
99">16'			H			H	H		100"
	L	G	I	I		L	L		L
			J			J	J		J
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Daylight ^P	Daylight ^P
Yes	if>16'	Yes ^K	Yes			Yes		No	
Yes	if>16'	Yes			Yes	Yes		Yes ^Y	Yes ^Q
	Yes			Yes	Yes	Yes		Yes	
Flash	Flash	Either							
Warning	Dot	Dot	Flash	None	^N	Flash	Flash	Flash	^R
^A	Yes	Yes	Yes	Yes	No	if>10'	Yes	Yes	^S
if>14'	if>16'	if>12'	if>10'	Yes ^{M V}	if>16'	if>12'	if>11'	Yes ^Y	^T
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

Current State Regulations for Transporting Manufactured Housing: National Network and Other Designated Highways

	AK	AZ	CA	CO	HI	ID	MT	NV	
Dimensions									
Width	A	A	4.87 m	A	4.26 m	4.87m	5.48 m	4.26 m	
Length (inc. tongue)	A	A	24.38 m	A		24.38			
Length (inc. tow vehicle)	A	A	32.00 m	A	21.33 m			29.50 m	
Height	A	A	A	A	A	A	A	A	
Tow Vehicle									
Wheelbase	2,540 mm	2,514 mm		N/A					
Gross Vehicle Weight	4,535 kg	B	C			D	L	F	
Axle Requirements	J	J				J	J	J	
Movement Restrictions									
Night	Yes	A	Yes	Yes ⁰	Yes	Yes	Yes	Yes	
Weekend	Yes	A	Yes	Yes ⁰	Yes	Yes	Yes	Yes	
Holiday	Yes	A	Yes	Yes ⁰		Yes	Yes		
Inclement Weather	Yes					Yes	Yes		
Requirements									
Lights	Dot					Solid		Flash	Flash
Lights	Beacon	Dot	Dot	Dot	Dot	Dot	Dot	Dot	
Signs	Yes	Yes	Yes	Yes	Yes	Yes	if>3.04m	Yes	
Pilot/Escort	if>3.04m ^x	Yes	Yes	Yes	if>3.65m	Yes	if>3.65m	Yes	
Permits	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	
Route Identification	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	

Footnotes:

- A Limited by route.
 B 1,360 kg - 3.04 m x 15.24 m; 1,814 kg - over 3.04 m x 15.24 m
 C 2,448 kg 2.43-3.65 m wide; 4,309 kg over 3.65 m
 D 3,628 kg 3.04 m - 3.65 m wide; 4,309 kg over 3.65 m
 E Plus .30 m eave
 F 680 kg less than 8,164 kg; 1,360 kg over 8,164 kg
 G 680 kg 3.04 m or less wide; 1,814 kg over 3.04 m
 H 2,260 mm cabover 3,048 mm conventional
 I 2,721 kg less than 3.04 m; 3,628 kg 3.04 m - 3.68 m; 4,082 kg over 3.65 m
 J 4 tires on driving axle
 K No movement on Sunday
 L 2,271 kg less than 3.04 m wide; 3,628 kg 3.04 m - 3.68 m; 4,082 kg over 3.65 m
 M Yes if > 4.87 m on Interstate or if > 6.09 m on other highways
 N Dot and rotating if over 3.65 m wide
 O Or limited by route.
 P 1/2 hour before sunrise to 1/2 after sunset
 Q Restricted on the following holidays as well as the day before and the day after: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day and Christmas.
 R Flashing or rotating beacon on top of tow vehicle, plus flashing or rotating beacon on roof at rear of manufactured home if escort vehicles no required.
 S Signs meeting AASHTO guide required if manufactured home over 2,590 mm wide
 T Escorts required when over 3.65 m wide on 2 lane hways and over 4.26 m wide on multi-lane hways. Escorts must be at least 1,524 mm wide and no larger than 1,587 kg pickup
 U Plus .60 m eaves.
 V Discretion of permit authority
 W 4.87 m base width allowed if hauled on licensed trailer
 X Or if overall length is greater than 25.90 m
 Y 4.26 m or > on 2 lane, 4.57 m or > on divided
 m meter
 mm ... millimeter
 kg kilogram

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
4.87 m	5.48 m	4.87 m	4.26 m	A	A	4.87 m	4.26 m A,E,W	A	4.26 m U
29.50 m			22.86 m	A					24.38 m
30.78 m	36.57 m		24.38 m	A	A		22.86 m		27.43 m
A	A	A	A	A	A	A	A	A	4.26 m
2,514mm>4.87m			H			H	H		2,540 mm
	L	G	I	I		L	L		L
			J			J	J		J
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Daylight P	Daylight P
Yes	if>4.87m	Yes K	Yes			Yes		No	
Yes	if>4.87m	Yes			Yes	Yes		Yes Y	Yes Q
	Yes			Yes	Yes	Yes		Yes	
Flash	Flash	Either				DOT			
Warning	Dot	Dot	Flash	None	N	Flash	Flash	Flash	R
A	Yes	Yes	Yes	Yes	No	if>3.04m	Yes	Yes	S
if>4.26m	if>4.87m	if>3.65m	if>3.04m	Yes M V	if>4.87m	if>3.68m	if>3.35m	Yes Y	T
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

Current State Regulations For Pilot/Escort Vehicles: National Network and Other Designated Highways

Vehicle & Load Dimensions For Which Pilot/Escort Are Required	AK	AZ	CA	CO	HI	ID	MT	NV
Two Lane Highway								
Width	10'	12' ^I	> 12' ^A	13'	NS	12' 1" ^C	12' 7"	12'
Length	85'	120'	^I	110' ^B	NS	100'	105' 1"	105' +
Height	NS	16'		16'	NS	16'		
Multi-Lane Highway								
Width	10'	14'	> 12' 1" ^A	15'	NS	15' 1"	16' 7" ^S	14'
Length	85'	120'	^I	115'	NS	120'	120' 1"	105' +
Height	NS	16'		16'	NS	16'		
Overhang								
Front	10' ^K		25' ^U					
Rear	20' ^P	20'	25'	25'				
Holiday Restrictions	Yes	Yes ^J	Yes	Yes	NS	Yes ^J	Yes ^J	Yes ^J
Manufactured Homes	Same	Same	Same	Same	NS	Same	Same	Same

Footnotes:

NS..... Not Specified

A Or when vehicle/load exceed lane width.

B 85' on 2 lane mountainous highways.

C 10' 1" on some mountainous highways.

D Or second person in cab.

E 12' in some situations.

F..... Depends on load.

G If over 16' on Interstate and over 20' on state system or discretion of permit authority.

H 16' wide 2 pilot cars' 108' long 2 pilot cars.

I Discretion of permit issuing authority.

J..... Holiday travel allowed with reduced dimensions

K If truck crane, then overhang can be up to 18'

L 12' wide, required pilot car.'

M 4' one pilot car, two on all over 14'

N As required for restricted overweight move or whenever load extends to the left of centerline.

O >16ft 1 escort, > 18ft 2 escorts

P >10' - 20' may travel w/o escort if all req lighting is extended

Q New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas.

R 12-14' one pilot car; 14-16' two pilot cars.

S Unless hwy is interstate the same requirements as two lane hwy

U Most vehicles limited to legal front overlong

T If escorts are required, shut down 1/2 hr. after sunset, day before holiday or holiday weekend resume 1/2 hr. before sunrise, day after holiday or holiday weekend.

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
13' 6"	if > 16'	14' ^E	N	^G	> 14'	14' ^H	11'	14'	^R
90' +	120'	80'	80'		> 110'	105' +	100'	110'	105'
16' ^D	18'	17'			> 17'	> 16'	14' 6"	^I	
14' +	if > 16'	12' ^F	14'	16' ^G	> 14'	> 14'	14'	15'	14'
90' +	120'	80'	140'		> 110'	> 120'	140'	^I	120'
16'	18'	17'			> 17'	16'	14' 6"	^I	
					> 20'	20'			
					> 20'	20'	1/3		20'
^I	Yes ^J	Yes	Yes ^J	No	Yes ^J	Yes	Yes	Yes ^T	Yes ^Q
Same	Same	^L	m	Same	^O	Same	Same	Same	Same

Current State Regulations For Pilot/Escort Vehicles: National Network and Other Designated Highways

Vehicle & Load Dimensions For Which Pilot/Escort Are Required	AK	AZ	CA	CO	HI	ID	MT	NV
Two Lane Highway								
Width	3.04 m	3.65 m ^I	> 3.68 m ^A	3.96 m	NS	3.68 m ^C	3.05 m	3.65 m
Length	25.90 m	36.57 m	^I	33.52 m ^B	NS	30.48 m	32.02 m	32.00 m
Height	NS	4.87 m		4.87 m	NS	4.87 m		
Multi-Lane Highway								
Width	3.04 m	4.26 m	> 3.68 m ^A	4.57 m	NS	4.59 m	5.05 m ^S	4.26 m
Length	25.80 m	36.57 m	^I	35.02 m	NS	36.57 m	36.60 m	32.00 m +
Height	NS	4.87 m		4.87 m	NS	4.87 m		
Overhang								
Front	3.04 m ^K		7.62 m ^U					
Rear	6.09 m ^P	6.09 m	7.62 m	7.62 m				
Holiday Restrictions								
	Yes	Yes ^J	Yes	Yes	NS	Yes ^J	Yes ^J	Yes ^J
Manufactured Homes								
	Same	Same	Same	Same	NS	Same	Same	Same

Footnotes:

m meter

kg kilograms

NS..... Not Specified

A Or when vehicle/load exceed lane width.

B 25.90 m on 2 lane mountainous highways.

C 3.07 m on some mountainous highways.

D Or second person in cab.

E 3.65 m in some situations.

F Depends on load.

G If over 4.87 m on Interstate and over 6.09 m on state system or discretion of permit authority.

H

I Discretion of permit issuing authority.

J Holiday travel allowed with reduced dimensions

K If truck crane, then overhang can be up to 5.48 m

L 3.65 m wide, required pilot car.¹

M 4.26 m one pilot car, two on all over 4.26 m

N As required for restricted overweight move or whenever load extends to the left of centerline.

O >4.87 m 1 escort, > 5.48 m 2 escorts

P >3.04 m - 6.09 m may travel w/o escort if all req lighting is extended

Q New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas.

R 3.65 m-4.26 m one pilot car; 4.26 m-4.87 m two pilot cars.

S Unless hwy is interstate the same requirements as two lane hways

T Most vehicles limited to legal front overlong

T If escorts are required, shut down 1/2 hr. after sunset, day before holiday or holiday weekend resume 1/2 hr. before sunrise, day after holiday or holiday weekend.

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
4.11 m	> 4.87 m	4.26 m ^E	N	G	> 4.26 m	4.26m	3.35 m	4.26 m	R
27.43 m	36.57 m	24.38 m	24.38 m		> 33.52 m	32.00+ m	30.48 m	33.52 m	32.00 m
4.87 m ^D	5.48 m	5.18 m			> 5.18 m	4.87 m	4.41 m	I	
4.26 m+	> 4.87 m	3.65 m ^F	4.26 m	4.87 m ^G	> 4.26 m	> 4.26 m	4.26 m	4.57 m	4.26 m
20.43 m+	36.57 m	24.38 m	42.67 m		> 33.52 m	> 36.57 m	42.67 m	I	36.57 m
4.87 m	5.48 m	5.18 m			> 5.18 m	4.87 m	4.41 m	I	
					6.09 m	6.09 m			
					6.09 m	6.09 m	1/3		6.09 m
I	Yes ^J	Yes	Yes ^J	No	Yes ^J	Yes	Yes	Yes ^T	Yes ^Q
Same	Same	L	m	Same	0	P	Same	Same	Same

Present State Regulations for Longer Combination Vehicles

	AK	AZ	CO	ID	MT	NV	ND	OK
Rocky Mountain Doubles								
Overall Length	120'	92'	NS	105'	95' ^D	105'	110'	NS ^G
Combined Trailer Length	95'	NS	NS	A	^D	NS	NS	110'
Semitrailer	48'	45'	48'	A	48'	48'	53'	53'
Trailer	48'	29'	28' 6"	A	NS	42'	53'	53'
Turnpike Doubles								
Overall Length	120'	105' ^I	NS	105'	100'	105'	110'	NS ^G
Combined Trailer Length	95'	NS	NS	A	NS	NS	NS	110'
Semitrailer	48'	NS	48'	A	NS	48'	53'	53'
Trailer	48'	NS	48'	A	NS	42'	53'	53'
Triple Trailer								
Overall Length	120' ^J	105'	NS	105'	105' ^C	105'	110'	NS ^G
Combined Trailer Length	NS	NS	NS	A	NS	NS	NS	95'
Individual Trailers	28' 6"	28' 6"	28' 6"	A	28' 6"	28' 6"	53'	29'
Maximum Gross Weight								
Uncapped		^I	110	105.5	Uncapped	129	80 ^K	80 ^E
(In 1000's kg)	BFB	124			BFB		105.5	90
Bridge Formula Exceptions								
(In 1000's kg)	None	None	36	9 Axles	9 Axles	None	None	None
			Tandem	Maximum	Maximum			
			Min. 6 Axles					
			Max. 9 Axles					

Footnotes:

NS Not Specified

N/A Not Allowed

IN Interstate

NIN Non Interstate

m meter

kg kilogram

A Must meet off-track rule max. off-track for 165' radius curve 6.5' on non-interstate routes 8.75' on interstate routes

B 95' for gas & LP transporters

C Conv. 110'

D 81' Combined trailer length. If operating under 81' combined trailer length 95' is not applicable

E 90,000 with permit on interstate & approved non int. rts

F Uncapped Formula B for rocky mountain doubles

G Interstate and 4-lane primary highways

H Must not exceed 8.75 off-track for 161' radius curve (inside front tire)

I Specified by route

J Only allowed during the summer and on one specific route

K By permit on Interstate over 80,000 lbs

L By permit on all state highways

OR	SD	TX	UT	WA	WY	WASHTO
N/A	NS	N/A	105'	L	NS	NS
N/A	81' 6"		81'NIN/95'IN	L	81'	81'
N/A	45'		48'	53'	48'	48'
N/A	NS		48'	53'	40'	NS
N/A	110'	N/A	105'	N/A	N/A	110'
N/A	NS		95' IN	N/A	N/A	95'
N/A	48' ^H		NS	N/A	N/A	45'
N/A	48' ^H		NS	N/A	N/A	45'
105'	110'	N/A	105'	N/A	N/A	NS
NS	NS		95'	N/A	N/A	NS
28' 6"	28' 6"		28' 6"	N/A	N/A	28' 6"
105	129 ^F	N/A	129	105.5	117	105.5 to
			BFB			129
None	None	N/A	None	None	36	NS
					Tandem	

Present State Regulations for Longer Combination Vehicles

	AK	AZ	CO	ID	MT	NV	ND	OK
Rocky Mountain Doubles								
Overall Length	86.57 m	28.04 m	NS	32.00 m	28.95 m ^D	32	33.52 m	NS ^G
Combined Trailer Length	28.95 m	NS	NS	A	D	NS	NS	33.52 m
Semitrailer	14.63 m	13.71 m	14.63 m	A	14.63 m	14.63 m	16.15 m	16.15 m
Trailer	14.63 m	8.83 m	8.68 m	A	NS	12.80 m	16.15 m	16.15 m
Turnpike Doubles								
Overall Length	36.57 m	32.00 m ^I	NS	32.00 m	30.48 m	32.00 m	33.52 m	NS ^G
Combined Trailer Length	28.95 m	NS	NS	A	NS	NS	NS	33.52 m
Semitrailer	14.63 m	NS	14.63 m	A	NS	14.63 m	16.15 m	16.15 m
Trailer	14.63 m	NS	14.63 m	A	NS	12.80 m	16.15 m	16.15 m
Triple Trailer								
Overall Length	36.57 m ^J	32.00 m	NS	32.00 m	32.00 m ^C	32.00 m	33.52 m	NS ^G
Combined Trailer Length	NS	NS	NS	A	NS	NS	NS	28.95 m
Individual Trailers	8.68 m	8.68 m	8.68 m	A	8.68 m	8.68 m	16.15 m	8.83 m
Maximum Gross Weight								
(In 1000's kg)	Uncapped BFB	I 56,018 kg	48,895 kg	47,853 kg	Uncapped BFB	58,513 kg	36,287 kg 47,853 kg	36,287 kg ^E 40,823 kg
Bridge Formula Exceptions								
(In 1000's kg)	None	None	16,329 kg Tandem Min. 6 Axles Max. 9 Axles	9 Axles Maximum	9 Axles Maximum	None	None	None

Footnotes:

- NS Not Specified
- N/A Not Allowed
- IN Interstate
- NIN Non Interstate
- m meter
- kg kilogram
- A Must meet off-track rule max. off-track for 50.29 m radius curve 1.98 m on non-interstate routes 2.66 m on interstate routes
- C Conv. 33.52 m
- D 24.68 m Combined trailer length. If operating under 24.68 m combined trailer length 95' is not applicable
- E 40,823 kg with permit on interstate & approved non int. rts
- F Uncapped Formula B for rocky mountain doubles
- G Interstate and 4-lane primary highways
- H Must not exceed 2.66 m off-track for 49.07 m radius curve (inside front tire)
- I Specified by route
- J Only allowed during the summer and on one specific route
- K By permit on Interstate over 36,287 kg
- L By permit on all state highways

OR	SD	TX	UT	WA	WY	WASHTO
N/A	NS	N/A	29.87	NS	NS	NS
N/A	24.84 m		24.68mNIN/ 28.95mIN	20.82 m	24.68 m	24.68 m
N/A	13.71		14.63 m	16.15 m	14.63 m	14.63 m
N/A	NS		14.63 m	16.15 m	12.19 m	NS
N/A	33.52 m	N/A	32.00 m	N/A	N/A	33.52 m
N/A	NS		28.95 m	N/A	N/A	28.95 m
N/A	14.63 m		14.63 m	N/A	N/A	13.71 m
N/A	14.63 m		14.63 m	N/A	N/A	13.71 m
32 m	33.52 m	N/A	32.00 m ^B	N/A	N/A	NS
NS	NS		28.95 m	N/A	N/A	NS
8.68 m	8.68 m		8.68 m	N/A	N/A	8.68 m
47,627 kg	58,513 kg ^F	N/A	58,513kg BFB	47,853 kg	53,070 kg	47,853 kg to 58,513 kg
None	None	N/A	None	None	16,329 kg Tandem	NS

State Regulations for Additional Longer Combination Vehicles

	AK	AZ	CA	CO	HI	ID	MT	NV
Full Truck & Trailer								
Overall Length	P	92'	65'		65' L	A & B	95'	
Truck Length	40'	NS	40'			NS	NS	
Trailer Length	48'	40'	40'		N/A	NS	NS	
Full Truck & Trailer & Trailer								
Overall Length	N/A	NS	65'	N/A	N/A	B & C	105'	
Combined Trailer Length		NS				NS	NS	
Trailer 1 Length		NS				NS	28' 6"	
Trailer 2 Length		NS				NS	28' 6"	
Dromedary Tractor & Trailer								
Overall Length	85' N	92'	N/A		N/A	B & C	NS	
Semitrailer Length	53'	NS		40'		NS	57'	
Dromedary Tractor & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	B & C	0	
Combined Trailer Length						NS	0	
Semitrailer Length						NS	48'	
Trailer Length						NS	NS	
Dromedary Tractor & Trailer & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	N/A	M	
Combined Trailer Length							NS	
Semitrailer Length							28' 6"	
Trailer Length							28' 6"	
Truck Tractor Pole Trailer & Pole Trailer								
Overall Length	120' N	92'	N/A	N/A	N/A	A,B,C	95'	
Combined Trailer Length	90'	NS				NS	NS	
Trailer Length	48'					NS	NS	
Maximum Gross Weight								
(in 1000's pounds)	Uncapped	111,000	80,000	110,000	80,000	105,500	Uncapped	
Bridge Formula Exceptions								
(in 1000's pounds)	None	None	None	J	Q	None	None	

Footnotes:

NS Not Specified

N/A .. Not Allowed

A Maximum length 85' with up to a 3' off track

B Maximum length 85' with up to a 5.5' offtrack C

C Maximum length 105' with an offtrack up to 6.5' on non interstate routes & up to 8.75' on interstate routes

D In a truck tractor, semitrailer, trailer configuration combined trailer length cannot exceed 81' including connecting mechanisms. Semi-trailer cannot exceed 48' and the trailer cannot exceed 40'. No overall length limitations apply to this combination

E 88' on designated highways

F 110' on the National Network only

G May only operate on the Interstate and four lane divided Fed-Aid primary system. LCV permit up to 90,000 lbs on Int.

H May operate on Interstate and four lane highways tractor & three trailers Four lane highways are limited to a 5 mile access

I 80,000 on interstate 90,000 off interstate

J Formula L + 40 X 800 (L=Length) not to exceed 20,000 on single axle nor 36,000 on tandem axles

K Longer vehicle combinations may operate by permit

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
N/A	75' ^E 50'	70'		80' ^K	N/A	77' ^R	75'	85'	
	53'	59' 6"		NS		48'	53'	60'	
N/A	75' ^{E&F} NS	N/A NS		NS 80'	N/A	95'	N/A N/A	85'	
	53'	NS		28.5'			N/A	60'	
	53'	NS		28.5'		48'	N/A	60'	
N/A	75' ^E 53'	NS 59' 6"		NS 53' ^K	N/A	77' ^R	NS 53'	85'	
N/A	75' ^{E&F} NS	NS 110' ^G		NS 81.5' ^K	N/A	N/A N/A	75' 68' ^S	85'	
	53'	53'		45'		48'	53'	60'	
	53'	53'		45'		48'	53'	60'	
N/A	75' ^{E&F} NS	NS 95' ^H		N/A N/A	N/A	N/A N/A	N/A N/A	N/A	
	53'	29'		N/A		N/A	N/A	N/A	
	53'	29'		N/A		N/A	N/A	N/A	
N/A	75' NS	N/A N/A		NS 81.5' ^K	N/A	92'	N/A 68'	85' ^D NS	
		N/A		45'			N/A	60'	
	105,000	^I		Uncapped		129,000	105,500	117,000	
	None	None		None		None	None	36,000 ^T	

L No limit on allowable length on FAP Hwys
M Triples operation, interstate only. Overall length is 105ft with cabover tractor, 3 trailers or full truck & 2 trailers; 110ft conventional tractor and three trailers
N Special designated routes
O 95ft combination length or 81ft combined trailer length, provided the longer trailer does not exceed 48ft
P Special designated routes 90ft overall without permit, other designated routes 85'
Q 88,000 other than interstate - Exception W=900 (L+40)

R May go up to 85' overall with offtrack approval
S By permit on all state highways
T Per tandem

State Regulations for Additional Longer Combination Vehicles

	AK	AZ	CA	CO	HI	ID	MT	NV
Full Truck & Trailer								
Overall Length	^P	28.04 m	19.81 m		19.81 m ^L	^{A & B}	28.95 m	
Truck Length	12.19 m	NS	12.19 m			NS	NS	
Trailer Length	14.63 m	12.19 m	12.19 m		N/A	NS	NS	
Full Truck & Trailer & Trailer								
Overall Length	N/A	NS	19.81 m	N/A	N/A	^{B & C}	32.00 m	
Combined Trailer Length		NS				NS	NS	
Trailer 1 Length		NS				NS	8.68 m	
Trailer 2 Length		NS				NS	8.68 m	
Dromedary Tractor & Trailer								
Overall Length	25.90 m ^N	28.04 m	N/A		N/A	^{B & C}	NS	
Semitrailer Length	16.15 m	NS		12.19 m		NS	17.37 m	
Dromedary Tractor & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	^{B & C}	0	
Combined Trailer Length						NS	0	
Semitrailer Length						NS	14.63 m	
Trailer Length						NS	NS	
Dromedary Tractor & Trailer & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	N/A	^M	
Combined Trailer Length							NS	
Semitrailer Length							8.68 m	
Trailer Length							8.68 m	
Truck Tractor Pole Trailer & Pole Trailer								
Overall Length	36.59 m ^N	28.04 m	N/A	N/A	N/A	^{A,B,C}	28.95 m	
Combined Trailer Length	27.45 m	NS				NS	NS	
Trailer Length	14.63 m					NS	NS	
Maximum Gross Weight								
(in 1000's pounds)	Uncapped	50,348 kg	36,287 kg	49,895 kg	36,287 kg	47,583 kg	Uncapped	
Bridge Formula Exceptions								
(in 1000's pounds)	None	None	None	^J	^Q	None	None	

Footnotes:

NS Not Specified

N/A .. Not Allowed

kg kilogram

m meter

A Maximum length 25.90 m with up to a .91 m off track

B Maximum length 25.90 m with up to a 1.67 m offtrack C

Maximum length 32.00 m with an offtrack up to 1.98 m on non

interstate routes & up to 2.66 m on interstate routes

D In a truck tractor, semitrailer, trailer configuration

combined trailer length cannot exceed 24.68 m including

connecting mechanisms. Semi-trailer cannot exceed 14.63 m and the trailer cannot exceed 12.19 m. No overall length limitations apply to this combination

E 26.82 m on designated highways

F 33.52 m on the National Network only

G May only operate on the Interstate and four lane divided Fed-Aid primary system. LCV permit up to 40,823 kg on Int.

H May operate on Interstate and four lane highways tractor & three trailers Four lane highways are limited to a 8.04 km access

NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
N/A	22.86 m ^E 15.24 m	21.33 m		24.38 m ^K	N/A	23.46 m ^R	22.86 m 12.19 m	25.90 m 18.28 m	
	16.15 m	18.13 m		NS		14.63 m	16.15 m	18.28 m	
N/A	22.86 m ^{E&F} NS	N/A NS		NS 24.38 m	N/A	28.95 m	N/A N/A	25.90 m NS	
	16.15 m	NS		8.68 m			N/A	18.28 m	
	16.15 m	NS		8.68 m		14.63 m	N/A	18.28 m	
N/A	22.86 m ^E 16.15 m	NS 18.16 m		NS 16.15 m ^K	N/A	23.46 m ^R	NS 16.15 m	55.90 m 18.28 m	
N/A	22.86 m ^{E&F} NS	NS 33.52 m ^G		NS 24.84 m ^K	N/A	N/A N/A	22.86 m 20.72 m ^S	25.90 m NS	
	16.15 m	16.15 m		13.71 m		14.63 m	16.15 m	18.28 m	
	16.15 m	16.15 m		13.71m		14.63 m	18.28 m	18.28 m	
N/A	22.86 m ^{E&F} NS	NS 28.95 m ^H		N/A N/A	N/A	N/A N/A	N/A N/A	N/A N/A	
	16.15 m	8.83 m		N/A		N/A	N/A	N/A	
	16.15 m	8.83 m		N/A		N/A	N/A	N/A	
N/A	22.86 m NS	N/A N/A		NS 24.84 m ^K	N/A	28.04 m	N/A 20.72 m	25.90 m ^D NS	
		N/A		13.71 m			N/A	18.28 m	
	47,853 kg	^I		Uncapped		58,513 kg	47,583 kg	53,070 kg	
	None	None		None		None	None	16,329 kg ^T	

I 36,287 kg on interstate 40,823 kg off interstate
J Formula L + 12.19 m X 800 (L=Length)not to exceed
9,071 kg on single axle nor 16,329 kg on tanden axles
K Longer vehicle combinations may operate by permit
L No limit on allowable length on FAP Hwys
M Triples operation, interstate only. Overall length is
32.00 m with cabover tractor, 3 trailers or full truck & 2
trailers; 33.62 m conventional tractor and three trailers
N Special designated routes

O 28.95 m combination length or 24.68 m
combined trailer length, provided the longer trailer
does not exceed 14.63 m
P Special designated routes 27.43 m overall
without permit, other designated routes 25.90 m
Q 39,916 kg other than interstate - Exception
W=900 (L+12.19 m)
R May go up to 25.90 m overall with offtrack
approval
S By permit on all state highways
T Per tandem

Vehicle Weight Limits in Canada

Gross Vehicle Weights	Yukon Territories		British Columbia		Alberta	
	Kg	Lbs	Kg	Lbs	Kg	Lbs
Truck 3 - Axles	26,900	59,304	26,100	57,540	24,300	53,572
Truck - Tandem/Tandem	34,900	76,941	34,000	74,957	30,600	67,461
Truck & Pony - 6 Axles	55,500	122,356	51,000	112,435	45,300	99,869
Truck & Full - 5 Axles	43,700	96,342	43,100	95,019	42,500	93,696
Truck & Full - 7 Axles	63,300	139,552	57,100	125,883	55,300	121,660
Tractor Semitrailer- 3 Axles						
- 4 Axles			31,600	72,311		
- 5 Axles	44,000	97,003				
- 6 Axles	54,000	119,049				
A Train- 5 Axles			38,000	83,775	41,900	92,373
- 6 Axles					49,800	109,789
- 7 Axles	63,500	139,993				
- 8 Axles	63,500	139,993				
Second Trailer Weight	NR					
B Train - 5 Axles			38,000	83,775		
- 6 Axles						
- 7 Axles	63,500	139,993				
- 8 Axles	63,500	139,993	63,500	63,500		
C Train* - 5 Axles						
- 6 Axles						
- 7 Axles	63,500	139,993			57,700	127,206
- 8 Axles	63,500	139,993	60,500		60,500	133,379
Second Trailer Weight	NR					
Axle Loads						
Steering Axle (Truck-Tractor)	9,000	19,841				
Single Axle Dual Tires	10,000	2,204				
Tandem - 1.2 M Spread	16,800	37,037				
Tandem - 1.8 M Spread	19,100	42,108				
Tridem - 2.4 To 3.0 M Spread	22,400	46,958	24,000	52,910		
Tridem - 3.0 To 3.6 M Spread	24,100	52,910	24,000	52,910		
Tridem - 3.7 M Spread	24,400					
Triaxle - 1.5 M + 2.5 M	27,900	61,508	NP		17,000	37,478
Triaxle - 1.8 M + 3.0 M	29,100	64,154	NP		17,000	37,478

* C Train Applicable Only to Combinations with CMVSS Approved C Dolly Converters

Legend:

- Blank . Same As Mou
- SP Special Permit Required
- NR Not Regulated
- NP Not Permitted
- Lbs Pounds
- Kg Kilograms

Saskatchewan		Manitoba		NW Territories	Mou	
Kg	Lbs	Kg	Lbs	Kg	Kg	Lbs
		24,300	53,572		22,500	49,604
28,000	61,729	SP		NR	NP	
		45,300	99,869	NP	43,500	95,901
40,700	89,728	41,300	91,050		39,500	87,082
		55,300	121,915		53,500	117,947
					23,700	52,249
					31,600	69,666
					39,500	87,082
					46,500	102,514
41,900	92,373	41,900	92,373	39,500	39,700	87,523
48,800	109,789	48,800	107,584		47,600	104,940
		55,500	122,356		53,500	117,947
		55,500	122,356		53,500	117,947
NR					16,000	35,273
					40,700	89,728
					48,600	107,144
					56,500	124,561
					62,500	137,788
					41,900	92,373
					49,800	109,790
53,500	117,947			58,500	54,600	120,372
		60,500	133,379		58,500	128,970
NR					21,000	46,297
					5,500	12,125
					9,100	20,062
					17,000	37,478
					17,000	37,478
					21,000	46,297
					23,000	50,706
				NR	24,000	52,910
NP		NP		NR	NP	
NP		NP		NR	NP	

Vehicle Dimension Limits in Canada

	Yukon Territories		British Columbia		Alberta		Saskatchewan	
	M	FT	M	FT	M	FT	M	FT
Overall Height (Max)	4.20	13.77						
Overall Width (Max)	2.60	8.50						
Overall Length (Max)								
Straight Truck								
Truck & Full Trailer	22.00	72.17						
Truck & Pony Trailer								
Tractor Semitrailer	23.00	72.17			23.00	75.45		
A Train	25.00	82.02	25.00	82.02	25.00	82.02	25.00	82.02
B Train	25.00	82.02	25.00	82.02	25.00	82.02	25.00	82.02
C Train	25.00	82.02	25.00	82.02	25.00	82.02	25.00	82.02
Trailer Length (Max)								
Full Trailer	16.20	44.29					16.20	53.14
Semitrailer	16.20	44.29	16.20	53.14	16.20	53.14	16.20	53.14
Box Length (Max)								
Truck & Full Or Pony Trailer	NR		20.00	65.60	20.00	65.62		
A Train	NR		18.50	60.69	20.00	65.62		
B Train	NR		20.00	65.60				
C Train	NR		18.50	60.69				
Effective Rear Overhang								
Straight Truck (Max)							NR	
Semitrailer (Max)								
Full Trailer (Max)			35% of WB				4.10	
Pony Trailer (Max)							4.10	
Wheelbase								
Tractor (Max)	NR							
Full Trailer (Min)	NR		6.25	20.50	6.25	20.50		
Semitrailer (Max)	NR							
Semitrailer (Min)	NR		6.25	20.50	6.25	20.50		
Semitrailer With Tridem (Min)	NR		6.25	20.50	6.25	20.50	6.50	21.32

Footnotes:

NA Not Available

Blank ... Same As MOU (*what is MOU?*)

MOU Memorandum of Understanding between Provinces

SP Special Permit Required

NR Not Regulated

NP Not Permitted

M Meters

FT Feet

WB Wheelbase

Manitoba		Northwest Territories		Mou
M	FT	M	M	FT
		4.20	4.15	13.61
2.60	8.50	3.20	2.60	8.50
25.00	82.02			
			12.50	41.01
		21.00	23.00	75.45
			23.00	75.45
		25.00	23.00	75.45
25.00	82.02	25.00	23.00	75.45
25.00	82.02	25.00	23.00	75.45
25.00	82.02	25.00	23.00	75.45
		NR	12.50	41.01
16.20	53.14	NR	16.20	53.14
		NR	20.00	65.62
18.50	60.69	NR	18.50	60.69
20.00	65.62	NR	20.00	65.62
20.00	65.62	NR	20.00	65.62
		NR	4.00	13.12
			35% of WB	
NR		NR	NR	
		NR	4.00	13.12
			6.20	20.34
6.25	20.50	NR	6.50	21.32
		NR	12.50	41.01
6.25	20.50	NR	6.50	21.32
6.25	20.50	NR	9.50	31.16




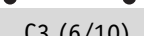
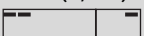


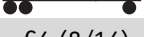
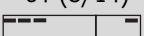
Vehicle Weight Limits in Mexico

Configuracion De Ejes Vehicle Configurations

			A4 Y A2 Metric Tons	Pounds
I—I 0	Sencillo Dos Llantas	Single Axle Single Tires	6.50	14,330
I—I 0	Motriz Sencillo Dos Llantas	Drive Axle Single Tires	7.00	15,432
II—II 0	Sencillo Cuatro Llantas	Single Axle Dual Tires	10.00	22,046
II—II 0	Motriz Sencillo Cuatro Llantas	Drive Axle Dual Tires	11.00	24,250
I—I 00 I—I	Doble 0 Tandem Cuatro Llantas	Dual Axle Single Tires	11.00	24,250
I—I 00 I—I	Motriz Doble 0 Tandem Cuatro Llantas	Dual Drive Axle Single Tires	12.50	27,557
II—II 00 I—I	Doble 0 Tandem Seis Llantas	Dual Axle 6 Tires	14.50	31,967
II—II 00 I—I	Motriz Doble 0 Tandem Seis Llantas	Dual Drive 6 Tires	15.50	34,171
II—II 00 II—II	Doble 0 Tandem Ocho Llantas	Dual Axle Dual Tires	18.00	39,683
II—II 00 II—II	Motriz Doble 0 Tandem Ocho Llantas	Dual Drive Axle Dual Tires	19.50	42,990
I—I 000 I—I I—I	Triple 0 Tridem Seis Llantas	Triple Axle Single Tires	14.00	30,864
I—I 000 I—I I—I	Motriz Triple 0 Tridem Seis Llantas	Triple Drive Axle Single Tires	15.50	34,171
II—II 000 II—II II—II	Triple 0 Tridem Doce Llantas	Triple Axle Dual Tires	22.50	49,604
II—II 000 II—II II—II	Motriz Triple 0 Tridem Doce Llantas	Triple Drive Axle Dual Tires	24.50	54,013

Tipo De Camino B4 Y B2 Metric Tons	Pounds	Road Type C Metric Tons	Pounds	D Metric Tons	Pounds
6.50	14,330	5.50	12,125	5.00	11,023
7.00	15,432	6.00	13,227	5.50	12,125
10.00	22,046	9.00	19,841	8.00	17,636
11.00	24,250	10.00	22,046	9.00	19,841
11.00	24,250	10.00	22,046	9.00	19,841
12.50	27,557	11.00	24,250	10.00	22,046
14.50	31,967	13.00	28,660	11.50	25,353
15.50	34,171	14.00	30,864	12.50	27,557
18.00	39,683	16.00	35,273	14.00	30,864
19.50	42,990	17.50	38,580	15.50	34,171
14.00	30,864	12.50	41,017	11.50	37,723
15.50	34,171	14.00	30,864	12.50	27,557
22.50	49,604	20.00	44,092	18.00	39,683
24.50	54,010	22.00	48,501	19.50	42,990

Camion / Full Truck

Configuración Del Vehículo Vehicle Configurations	Numero De Llantas	Number of Tires	A4 Y A2		B4 Y B2	
			Metric Tons	Pounds	Metric Tons	Pounds
 C2 (4/6)	Cuatro Llantas	4	13.50	29,762	13.50	29,762
 	Seis Llantas	6	17.50	38,580	17.50	38,580
 C3 (6/10)	Seis Llantas	6	19.00	41,887	19.00	41,887
 	Diez Llantas	10	26.00	57,320	26.00	57,320
 C4 (8/14)	Ocho Llantas	8	22.00	48,501	22.00	48,501
 	Catorce Llantas	14	31.00	68,343	31.00	68,343

Camion Remolque / Full Truck and Trailer

Configuración Del Vehículo Vehicle Configurations	Numero De Llantas	Number of Tires	A4 Y A2		B4 Y B2	
			Metric Tons	Pounds	Metric Tons	Pounds
 C2-R2 (8/14)	Ocho Llantas	8	26.50	58,422	26.50	58,422
 	Catorce Llantas	14	37.50	82,673	37.50	82,673
 C3-R2 (10/18)	Diez Llantas	10	32.00	70,547	32.00	70,547
 	Dieciocho Llantas	18	46.00	101,412	46.00	101,412
 C3-R3 (12/22)	Doce Llantas	12	36.50	80,468	36.50	80,468
 	Vientidos Llantas	22	54.00	119,049	54.00	119,049

Footnotes:

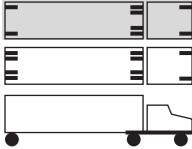
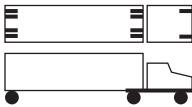
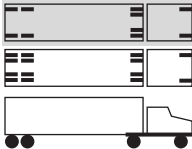


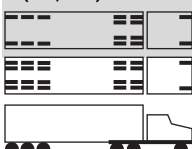

El Peso Bruto Vehicular (Pbv) Maximo Autorizado Podra Incrementarse Hasta Un Cinco Porciento (5%), Si El Vehiculo Esta Equipado Con Suspension Neumatic O Equivalente En Todos Sus Ejes, Excepto En El Eje Direccional.

Road Type			
C		D	
Metric Tons	Pounds	Metric Tons	Pounds
11.50	25,353	11.00	24,250
15.50	34,171	14.00	30,864
16.50	36,376	15.00	33,069
23.00	50,706	20.50	45,194
19.50	42,990	17.50	38,580
27.50	60,627	24.50	54,013

Road Type		
C		D
Metric Tons	Pounds	
22.50	49,604	N/A
33.50	73,854	N/A
27.50	60,627	N/A
41.00	90,389	N/A
32.00	70,547	N/A
48.00	105,821	N/A

The gross vehicle weight authorized can be increased 5% if the vehicle is equipped with pneumatic suspension or equivalent on all its axles except the steer axle

Tractocamion Articulado / Truck Tractor & Trailer

Configuración Del Vehículo Vehicle Configurations	Numero De Llantas	Number of Tires	Tipo De Camino / Road Type	
			A4 Y A2 Metric Tons	Pounds
T2-S1 (8/10) 	OCHO LLANTAS	8	24.00	52,910
	DIEZ LLANTAS	10	27.50	60,627
T2-S2 (10/14) 	DIEZ LLANTAS	10	28.50	62,831
	CATORCE LLANTAS	14	35.50	78,264
T3-S2 (18) 	DIECIOCHO LLANTAS	18	44.00	97,003
T3-S3 (16/22) 	DIECISEIS LLANTAS	16	40.00	88,184
	VEINTIDOS LLANTAS	22	48.50	88,184

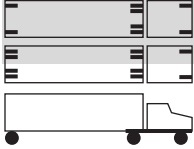
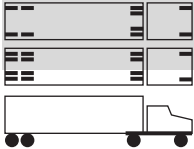
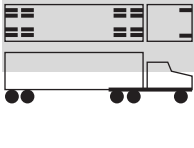

Footnotes:

El peso bruto vehicular (pbv) máximo autorizado podrá incrementarse hasta un cinco por ciento (5%), si el vehículo está equipado con suspensión neumática o equivalente en todos sus ejes, excepto en el eje direccional.

The gross vehicle weight authorized can be increased 5% if the vehicle is equipped with pneumatic suspension or equivalent on all its axles except the steer axle

Tipo De Camino / Road Type					
B4 Y B2		C		D	
Metric Tons	Pounds	Metric Tons	Pounds	Metric Tons	Pounds
24.00	52,910	21.00	46,297	N/A	
27.50	60,627	24.50	54,013	N/A	
28.50	62,831	25.50	56,217	N/A	
35.50	78,264	31.50	69,445	N/A	
44.00	97,003	39.00	85,980	N/A	
40.00	88,184	35.50	78,264	N/A	
48.50	106,924	43.00	94,798	N/A	

Tractocamion Articulado / Truck Tractor & Trailer

Configuración Del Vehículo Vehicle Configurations	Tipo De Vehículo	Type of Vehicle	Tipo De Camino / Road Type	
			A4 Y A2	FT
			M	
	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Tres Ejes	3 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33
	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Cuatro Ejes	4 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33
	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Cinco Ejes	5 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33
	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Cinco Ejes	6 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33






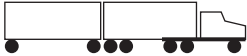


Footnotes:

A Ancho Del Vehículo W Width of Vehicle
L Longitud Del Vehículo L Length of Vehicle
H Altura Del Vehículo 4.15 M H Height 13.61 Ft

Tipo De Camino / Road Type				
B4 Y B2 M	FT	C M	FT	D M
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A

Tractocamion Doblemente Articulado / Truck Tractor & Trailer & Trailer

Configuración
Del Vehículo
Vehicle
Configurations

	Tipo De Vehículo	Type of Vehicle	Tipo De Camino / Road Type	
			A4 Y A2 M	FT
T2-S1-R2 (12/18)  	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Cinco Ejes	5 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22
T3-S1-R2 (16/22)  	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Seis Ejes	6 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22
T3-S2-R2 (18/26)  	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Seite Ejes	7 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22
T3-S2-R4 (22/34)  	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Nueve Ejes	9 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22

Footnotes:

A Ancho Del Vehículo

L Longitud Del Vehículo

H Altura Del Vehículo 4.15 M

W Width of Vehicle

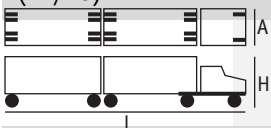
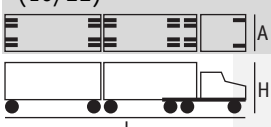
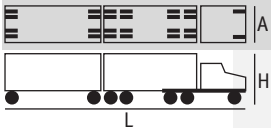
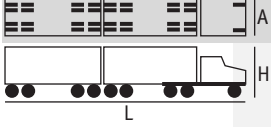
L Length of Vehicle

H Height 13.61 Ft

Tipo De Camino / Road Type				
B4 Y B2 M	FT	C M	FT	D M
A = 2.50 L =24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A
A = 2.60 L =23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A
A = 2.50 L =24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A
A = 2.60 L =23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A
A = 2.50 L =24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A
A = 2.60 L =23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A
A = 2.50 L =24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A
A = 2.60 L =23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A

Tractocamion Doblemente Articulado / Truck Tractor and Trailer and Trailer

**Configuración
Del Vehículo
Vehicle
Configurations**

	Numero De Llantas	Number of Tires	Tipo De Camino / Road Type	
			A4 Y A2 Metric Tons	Pounds
T2-S1-R2 (12/18) 	DOCE LLANTAS	12	37.00	81,571
	DIECIOCHO LLANTAS	18	47.50	104,719
T3-S1-R2 (16/22) 	DIECISEIS LLANTAS	16	45.50	100,310
	VEINTIDOS LLANTAS	22	56.00	123,458
T3-S2-R2 (18/26) 	DICIOCHO LLANTAS	18	50.00	110,231
	VEINTISEIS LLANTAS	26	60.50	133,379
T3-S2-R4 (22/34) 	VEINTIDOS LLANTAS	22	59.00	130,072
	TREINTA Y CUATRO LLANTAS	34	66.5*	146,607

Footnotes:

El peso bruto vehicular (pbv) maximo autorizado podra incrementarse hasta un cinco por ciento (5%), si el vehiculo esta equipado con suspension neumatica o equivalente en todos sus ejes, excepto en el eje direccional.

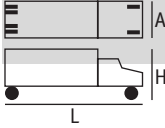
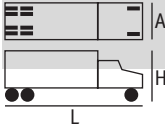
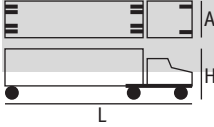
The gross vehicle weight authorized can be increased 5% if the vehicle is equipped with pneumatic suspension or equivalent on all its axles except the steer axle

* el peso bruto vehicular para este tipo de unidades que trasladan gases o quimicos por caminos tipo "a" sera de 72.5 ton. Por un periodo de 5 años a partir de la expedición del presente reglamento, posteriormente a este periodo deberan ajustarse al valor endicado.

The unit's gross vehicle weight, that transports gases and chemicals, over road type a would be 72.5 ton. For a period of 5 years as of this rule, before this rule you should adjust the weights indicated.

Tipo De Camino / Road Type					
B4 Y B2		C		D	
Metric Tons	Pounds	Metric Tons	Pounds	Metric Tons	Pounds
37.00	81,571	32.00	70,547	N/A	
47.50	104,719	42.50	93,696	N/A	
45.50	100,310	39.50	87,082	N/A	
55.00	121,254	50.00	110,231	N/A	
50.00	110,231	44.00	97,003	N/A	
58.00	127,868	52.50	115,742	N/A	
59.00	130,072	53.00	116,845	N/A	
64.00	141,095	58.00	127,868	N/A	

Camion / Full Truck

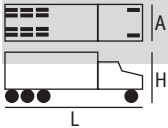
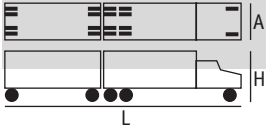
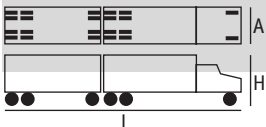
Configuración Del Vehículo Vehicle Configurations	Tipo De Vehículo	Type of Vehicle	Tipo De Camino / Road Type	
			A4 Y A2 M	FT
C2 (4/6) 	Camion	Full Truck	A = 2.50 L = 14.00	W = 8.20 L = 45.93
	Dos Ejes	2 Axles	A = 2.60 L = 13.71	W = 8.53 L = 44.98
C3 (6/10) 	Camion	Full Truck	A = 2.50 L = 14.00	W = 8.20 L = 45.93
	Tres Ejes	3 Axles	A = 2.60 L = 13.71	W = 8.53 L = 44.98
C4 (8/14) 	Camion	Full Truck	A = 2.50 L = 14.00	W = 8.20 L = 45.93
	Cuatro Ejes	4 Axles	A = 2.60 L = 13.71	W = 8.53 L = 44.98

Footnotes:

A Ancho Del Vehículo	W Width of Vehicle
L Longitud Del Vehículo	L Length of Vehicle
H Altura Del Vehículo 4.15 M	H Height 13.61 Ft

Tipo De Camino / Road Type					
B4 Y B2 M	FT	C M	FT	D M	FT
A = 2.50 L = 14.00	W = 8.20 L = 45.93	A = 2.50 L = 14.00	W = 8.20 L = 45.93	A = 2.50 L = 12.50	W = 8.20 L = 41.01
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	A = 2.60 L = 12.25	W = 8.53 L = 40.19
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	A = 2.50 L = 12.50	W = 8.20 L = 41.01
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	A = 2.60 L = 12.25	W = 8.53 L = 40.19
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	A = 2.50 L = 12.50	W = 8.20 L = 41.01
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	A = 2.60 L = 12.25	W = 8.53 L = 40.19

Camion Remolque / Full Truck & Trailer

Configuración Del Vehículo Vehicle Configurations	Tipo De Vehículo	Type of Vehicle	Tipo De Camino / Road Type	
			A4 Y A2 M	FT
C2-R2 (8/14) 	Camion Remolque	Full Truck	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Cuatro Ejes	4 Axles	A = 2.60 L = 27.00	W = 8.53 L = 88.58
C3-R2 (10/18) 	Camion Remolque	Full Truck	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Cinco Ejes	5 Axles	A = 2.60 L = 27.00	W = 8.53 L = 88.58
C4-R3 (12/22) 	Camion Remolque	Full Truck	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Seis Ejes	6 Axles	A = 2.60 L = 27.00	W = 8.53 L = 88.58

Footnotes:

A Ancho Del Vehículo W Width of Vehicle
L Longitud Del Vehículo L Length of Vehicle
H Altura Del Vehículo 4.15 M H Height 13.61 Ft

Tipo De Camino / Road Type					
B4 Y B2 M	FT	C M	FT	D M	FT
A = 2.50 L = 23.50	W = 8.20 L = 77.09	A = 2.50 L = 22.50	W = 8.20 L = 73.81	N/A	
A = 2.60 L = 27.00	W = 8.53 L = 73.81	A = 2.60 L = 21.50	W = 8.53 L = 70.53	N/A	
A = 2.50 L = 23.50	W = 8.20 L = 77.09	A = 2.50 L = 22.50	W = 8.20 L = 73.81	N/A	
A = 2.60 L = 27.00	W = 8.53 L = 73.81	A = 2.60 L = 21.50	W = 8.53 L = 70.53	N/A	
A = 2.50 L = 23.50	W = 8.20 L = 77.09	A = 2.50 L = 22.50	W = 8.20 L = 73.81	N/A	
A = 2.60 L = 27.00	W = 8.53 L = 73.81	A = 2.60 L = 21.50	W = 8.53 L = 70.53	N/A	

SUPERLOADS MAXIMUM ROUTINE PERMIT DIMENSIONS



Permit requests that exceed any of these dimensions and weights may require special handling of the permit. See the following pages for procedures on superload alert.

SUPERLOADS

California:

Special handling for width over 15 ft.

Maximum weights allowed on "purple chart" may not be exceeded. Additional axles must be added or weight of the load reduced to conform to "purple chart loading." No exceptions are made. Acceptable hauling equipment should be used.

Hawaii:

Special handling and review by engineering is required if the following is exceeded:

Tread width of 8 feet, over 42,000 on a single axle.

Tread width of 8 feet, over 56,000 on a tandem.

Tread width of 9 feet, over 45,000 on a single axle.

Tread width of 9 feet, over 60,000 on a tandem.

Tread width of 10 feet, over 48,000 on a single axle.

Tread width of 10 feet, over 64,000 on a tandem.

Washington:

Special handling for height. It is the responsibility of the permittee to carefully plan the route. Some structures on state highways do not have a 14-foot clearance; many have less than 14 feet 6 inches. All movements in excess of 14 feet 6 inches shall be accompanied by an escort vehicle, which shall precede the permittee to provide warning to traffic and advise permittee of hazardous conditions.

SUPERLOAD ALERT PROCEDURES

Using the criteria in the WASHTO Guide on pages 57 and 58, the WASHTO State receiving an inquiry or request for a permit, which exceeds maximum routine permit dimensions, should use the following procedure to assure that the appropriate States are notified of a possible “superload” move.

- 1) If the movement is a “superload” in one or more States, complete the checklist on the fax transmittal sheet.
- 2) Advise the carrier that dimensions and/or weights require special handling by one or more of the States. The carrier should be advised to contact each State. Provide a contact name, phone number and fax number for each State.
- 3) Send the completed fax transmittal sheet to each State involved in the move.

Note: WASHTO Border states may wish to add their neighboring state even if that state is not a WASHTO member.

The following is a list of phone numbers and fax numbers for the WASHTO States Permit Offices.

PERMIT OFFICES	Phone No.	Fax No.
Alaska, Anchorage	(907) 345-7636	(907) 345-2641
Arizona, Phoenix	(602) 255-7346	(602) 269-5611
California, Sacramento	(916) 654-4828	(916) 654-3498
Colorado, Denver	(303) 757-9539	(303) 757-9719
Idaho, Boise	(208) 334-8420	(208) 334-8419
Montana, Helena	(406) 444-6130	(406) 444-7670
Nevada, Carson City	(702) 888-7410	(702) 888-7103
New Mexico, Santa Fe	(505) 827-0376	(505) 983-1565
North Dakota, Bismark	(701) 328-2621	(701) 328-1642
Oklahoma, Oklahoma City	(405) 425-2390	(405) 425-2811
Oregon, Salem	(503) 945-7903	(503) 945-7945
South Dakota, Pierre	(605) 698-3925	(605) 698-7665
Texas, Austin	(800) 299-1700	(512) 465-3575
Utah, Salt Lake City	(801) 965-4508	(801) 965-4936
Washington, Olympia	(360) 664-9494	(360) 664-9440
Wyoming, Cheyenne	(307) 777-4376	(307) 777-4399

MODEL PILOT/ESCORT VEHICLE REQUIREMENTS

- 0-1 The operator of a pilot/escort vehicle must comply with all applicable traffic laws and with the requirements of these Rules when escorting a vehicle or load on a state highway, in order to protect the safety of highway users and to protect the efficient movement of traffic from unreasonable interference.
- 0-2 The operator of a pilot/escort vehicle must obtain and maintain certification as provided in this chapter to be authorized to escort an extra-legal vehicle or load on a state highway. The permittee of the extra-legal vehicle or load shall be responsible to inspect a pilot/escort vehicle operator's certification to ensure that it is valid and current. To obtain a certification, the operator must:
1. Submit an application for certification to the Department, in the form required by the Department. The application must contain all required information and documentation.
 2. Receive a minimum of eight (8) hours of training specific to escort requirements, including principals of defensive driving.
 3. Possess a valid motor vehicle driver's license.
 4. Possess the minimum insurance as required by the state where the escort vehicle is licensed.
- 0-3 The pilot/escort vehicle certification examination will consist of pilot car escort vehicle operation requirements, including:
1. Requirements applicable to an extra-legal vehicle or load, responsibilities of the operator when escorting an extralegal vehicle and/or load, and pilot/escort vehicle procedures, all as described in the Rules; and
 2. Requirements of applicable traffic laws.
 3. Procedures for operator flagging and traffic control, as described in Section 6 E of the Manual on Uniform Traffic Control Devices ("M.U.T.C.D"), Part 6, Published September 3, 1993.
- 0-4 The certifying state may set a fee to cover administrative costs.
- 0-5 The Department will grant a certificate to all persons who pass the pilot/escort vehicle certification examination and satisfy the other requirements of Section 0-2 above.
- The certification must be in the possession of the pilot/escort vehicle operator at all times when the operator is escorting an extra-legal vehicle or load on a state highway. The certification shall be exclusive to the operator named in the certificate and may not be assigned or transferred.
- 0-6 The term of the certification shall be for a maximum period of 4 years, subject to Section 0-7 below, and shall expire automatically 4 years after the date of issuance by the Department. Operators must notify the Department in writing within 30 days of any change of address or name. Upon expiration of the certification, an operator must again comply with the requirements of Section 0-2 of the Rules before the Department will issue a new certificate.
- 0-7 The certification shall be deemed automatically canceled, suspended or revoked for:
1. Failure to satisfy the requirements of Section 0-2 above, or failure to give the required or correct information on the application for certification as a pilot/escort vehicle operator, or the commission of any fraud in making the application.
 2. Violation of rules established by the certifying state.
 3. Failure to notify the Department of any cancellation, suspension, or revocation of the operator's motor vehicle driver's license by the 'DMV' within five working days of the operator's receipt of notice of the same.
 4. Conduct while operating a motor vehicle or a pilot/escort vehicle, which, as determined by the Department, threatens the public safety or welfare of highway users or creates unreasonable interference with the efficient movement of traffic.
 5. Allowing or permitting an uncertified person to operate a pilot/escort vehicle under the operator's supervision or direction.

0-8 When the pilot/escort vehicle is in front of the extra-legal vehicle or load being escorted, the operator shall:

1. Warn oncoming traffic of the presence of the vehicle or load by use of signs and lights as provided in Sections 0-11 and 0-15 of the Rules.
2. Notify the driver of the extra-legal vehicle or load by two-way radio of all hazards; overhead clearances; obstructions; traffic congestion; pedestrians; and any other circumstances evident to the operator that could affect either the safe movement of the extra-legal vehicle and/or load, the safety of the traveling public, or the efficient movement of traffic in sufficient time for the driver of the extra-legal vehicle and/or load to take remedial action, as necessary.
3. To the extent necessary, locate safe places (if available) adjacent to the highway and notify the driver of the extralegal vehicle or load thereof to allow the extra-legal vehicle and/or load and the pilot/escort vehicle(s) to clear the highway, so that following traffic can safely pass or for any other reasons necessary to protect public safety and the efficient movement of traffic.
4. Be far enough in front of the extra-legal vehicle and/or load to signal oncoming motorists to stop in a timely manner, or as specified by local jurisdiction, before such motorists enter any narrow structures or other restrictions on the highway to permit the safe passage of the vehicle or load

0-9 When the pilot/escort vehicle is behind the extra-legal vehicle or load, the operator shall:

1. Warn traffic approaching from the rear of the presence of the extra-legal vehicle or load ahead, by use of signs and lights as provided in Section 0-11 and 0-15 of the Rules.
2. Notify the driver of the extra-legal vehicle or load by two-way radio of flat tires or other problems with the extralegal vehicle and/or load; objects coming loose from the extralegal vehicle and/or load; other traffic approaching or passing the extra-legal vehicle and/or load; and any other circumstances evident to the operator that could affect either the safe movement of the extra-legal vehicle and/or load, the safety of the traveling public, or the efficient movement of traffic in sufficient time for the driver of the extra-legal vehicle and/or load to take remedial action, as necessary.
3. Notify the front pilot/escort driver and the driver of the extra-legal vehicle and/or load by two-way radio of traffic build-up and other delays to the normal flow and efficient movement of traffic caused by the movement of the extralegal vehicle or load.
4. Notify the driver of the vehicle and/or load by two-way radio of other vehicles attempting to pass the extra-legal vehicle and/or load.
5. Be far enough behind the extra-legal vehicle and/or load to signal motorists following the extra-legal vehicle and/or load to slow or stop in a timely manner, or as specified by local jurisdiction, before narrow structures or other restrictions in the highway to permit the safe passage of the extra-legal vehicle and/or load.

0-10 The pilot/escort vehicle operator shall ensure that the pilot/escort vehicle is in safe operating condition at all times when the pilot/escort vehicle is escorting an extra-legal vehicle or load.

0-11 A pilot/escort vehicle shall, in addition to any other equipment required by the traffic laws, be equipped with at least one, but not more than three, flashing yellow lights, as defined in Rules. The flashing yellow light(s) shall be visible to approaching traffic from the front and the rear of the pilot/escort vehicle.

0-12 The pilot/escort vehicle shall:

1. Be either a passenger car or a two-axle truck;
2. Not exceed a maximum gross vehicle weight rating of 14,000 pounds;
3. Be at least 60 inches wide; and
4. Not exceed the legal limits of size and weight, as defined in Rules.

- 0-13 The pilot/escort operator shall not carry any item(s) or equipment or load in or on the pilot car escort vehicle which:
1. Exceeds the height, length, or width of the pilot car escort vehicle, or overhangs the pilot/escort vehicle, or otherwise impairs its immediate recognition as a safety pilot/escort vehicle by the motoring public; or
 2. Obstructs the view of the flashing yellow lights or the signs used by the pilot/escort vehicle; or
 3. Causes safety risks; or
 4. otherwise impairs the performance by the operator or the pilot/escort vehicle of the duties required by the Rules.
- 0-14 The pilot/escort vehicle operator shall properly load and secure any item(s) or equipment or load carried by the pilot/escort vehicle to ensure compliance with the requirements of this Section.
- 0-15 A pilot/escort vehicle shall display "Wide Load", "Long Load", or "Oversize Load" signs, as applicable, which shall be mounted above the roofline of the escort vehicle and be visible to approaching traffic from the front and the rear. All such signs shall be a minimum of five feet wide, ten inches high with one inch wide brush stroke, black letters a minimum of eight inches high on yellow background, or shall be a maximum of seven feet wide, eighteen inches high, with a 1.41-inch brush stroke, black letters a minimum of ten inches high on yellow background.
- 0-16 The pilot/escort vehicle shall use its headlights at all times when escorting an extra-legal vehicle or load.
- 0-17 The pilot/escort vehicle shall be equipped with two-way radio facilities capable of providing reliable voice communication both ways between the driver of the extra-legal vehicle and/or load and the driver of the pilot/escort vehicle at all times when the extra-legal vehicle and/or load is in motion on a state highway.
- 0-18 A pilot/escort vehicle shall carry the following items of equipment at all times when escorting an extra-legal vehicle or load:
1. Standard 18 inch STOP & SLOW paddle sign.
 2. Three bi-directional emergency reflective triangles.
 3. A minimum of one 5 pound B,C, fire extinguisher.
 4. A reflectorized orange vest, shirt or jacket, which must be worn by the operator while directing traffic.
 5. An orange or white hard hat, which must be worn by the operator when out of the vehicle.
 6. Two additional signs, as described in Section 0-15.
 7. A height measuring device which is non-conductive and non-destructive to overhead clearances, when required by the terms of the permit or regulations.
 8. A first aid kit.
 9. A flashlight with a red cone and extra batteries.
- 0-19 A pilot/escort vehicle is prohibited from escorting more than one extra-legal vehicle or load at the same time, unless expressly so authorized.
- 0-20 A pilot/escort vehicle shall use a height pole at all times when escorting an extra-legal vehicle or load exceeding sixteen feet in height, unless otherwise expressly authorized by the Department on the permit. The height pole shall not extend less than six inches above the maximum height of the extra-legal vehicle and/or load the pilot/escort vehicle is escorting. When the pilot/escort vehicle is not escorting an extra-legal vehicle and/or load but is moving on the highway, the height pole shall be removed, tied down, or shortened to within legal limits.

0-21 Identification signs or placards shall be displayed on the pilot/escort vehicle at all times when escorting an extra-legal vehicle and/or load. The sign or placard must:

1. Appear on both sides of the pilot/escort vehicle.
2. Be in letters that contrast sharply in color with the background on which the letters are placed.
3. Be readily legible during daylight hours from a distance of 50 feet while the vehicle is stationary; and
3. Be kept and maintained in a manner that retains the legibility required by subparagraph 3 of this Section.

0-22 The operator of the pilot/escort vehicle shall use the pilot/escort vehicle and the equipment described herein only in compliance with the rules.

0-23 When a pilot/escort vehicle is not escorting an extra-legal vehicle and/or load but is moving on the highway, the signs described in Section 0-15 of the Rules shall either be removed or covered, and the flashing yellow lights described in Section 0-11 of the Rules shall not be operated.

0-24 In the performance of the duties required by the Rules, the operator of the pilot/escort vehicle may direct other traffic to stop, slow, or proceed in situations where such direction is necessary to allow the extra-legal vehicle or load to continue moving safely, except as provided below.

The operator of the pilot/escort vehicle shall signal the extra-legal vehicle and/or load to stop, and the extra-legal vehicle shall stop, as far off of the roadway as practicable to allow other traffic to pass the stopped extra-legal vehicle and/or load in the following situations:

1. When the extra-legal vehicle and/or load becomes disabled; or
2. When the movement of the extra-legal vehicle and/or load on a particular section of state highway presents a safety risk or unreasonably interferes with the efficient movement of other traffic, based upon such factors as the widths of the extra-legal vehicle and/or load and the roadway, volume of other traffic, visibility and limited sight distance, and mountainous terrain; or
3. When driving conditions for the extra-legal vehicle and/or load are hazardous for any other reason, including weather. When the extra-legal vehicle and/or load has been stopped pursuant to this Subsection 3, the pilot/escort operator shall then direct other traffic past the extra-legal vehicle and/or load, as necessary, until such time as the extra-legal vehicle and/or load can re-enter the roadway and continue moving without presenting a safety risk or unreasonably interfering with the efficient movement of other traffic.

When directing traffic in the situations described in this Section, the operator shall comply with the flagging procedures and requirements described in Section 6 E of the M.U.T.C.D, Part 6, and Published September 3, 1993.

1. Stand outside the pilot/escort vehicle;
2. Use the paddle signs and other equipment identified in Section 0-18; and

0-25 The operator of the extra-legal vehicle or load and the operator of the pilot/escort vehicle shall comply with the following procedures:

1. Before trip:

- A. Discuss aspects of the move, including the extra-legal vehicle and/or load, the route, and specific responsibilities.
- B. Review permit conditions.
- C. Review the permitted route.
- D. Determine the proper position of the pilot/escort vehicle(s).
- E. Establish particular procedures.
- F. Check mandatory equipment
- G. Mount signs, adjust mirrors, turn on lights.
- H. Check each 2-way radio to ensure clear communication on a selected channel.
- I. Verify that drivers licenses, extra-legal vehicle and/or load transport permit, and pilot car escort vehicle certification are in the possession of the operators.
- J. Determine if additional flagpersons will be necessary and, if so, have them available.

2. During trip:

- A. Obey all traffic laws.
- B. Maintain proper distance between the extra-legal vehicle and/or load and the pilot/escort vehicle:
 - 1. Do not follow or precede more closely than is reasonably prudent, considering the speed of the extra-legal vehicle and/or load, other traffic, and highway conditions.
 - 2. Do not exceed 1/2-mile distance between extra-legal vehicle and/or load and the pilot/escort vehicle to maintain radio contact.
 - 3. Be close enough to warn other traffic of extra-legal vehicle or load.

3. Traffic lights:

- A. If the pilot/escort vehicle goes through a traffic light but the extra-legal vehicle and/or load does not, then the pilot/escort vehicle must pull over to the right side of the highway, where practicable, to wait for the extra-legal vehicle and/or load.
- B. If the extra-legal vehicle and/or load goes through the traffic light but the pilot/escort vehicle does not, then the extra-legal vehicle and/or load must continue and the pilot/escort vehicle must catch up when possible.

WESTERN ASSOCIATION OF
STATE HIGHWAY AND TRANSPORTATION OFFICIALS

RESOLUTION 90-1

TITLE: RESOLUTION TO ADOPT A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT AMONG THE WASHTO STATES

WHEREAS, the regional approach to truck size and weight uniformity is needed because of similarities among WASHTO states; and

WHEREAS, the Executive Committee passed a resolution that recognizes and endorses the need for uniformity of truck size and weight laws and regulations among the WASHTO states; and

WHEREAS, the Subcommittee on Highway Transport was created by WASHTO to identify and discuss major motor carrier industry related transportation issues within the WASHTO region; and

WHEREAS, the mission of the Subcommittee on Highway Transport is to define actions and solutions, improve coordination and communications, and to recommend policies to the WASHTO Standing Committee on Engineering and Operations; and

WHEREAS, the Subcommittee on Highway Transport maintains continuing liaison with representatives of the trucking industry to improve coordination and understanding between state highway and transport industry officials and to promote uniformity on various industry-related transportation issues; and

WHEREAS, the Subcommittee on Highway Transport has developed in conjunction with the Western Trucking Association Executive Council and the Western Highway Institute initial recommendations as a guide for the operation of legal vehicles, vehicles transporting over dimensional loads, extra-legal weight vehicles, and longer combination vehicles; and

WHEREAS, the Subcommittee on Highway Transport finds it in the best interest on fulfilling its mission to make this guide a continuing and dynamic document; and

WHEREAS, the concept of uniformity embodied in the guide contemplates minimum objectives which should realistically be obtainable in all of the WASHTO states, but also recognizes that higher goals may be more appropriate to the needs of individual states; and

NOW, THEREFORE, BE IT RESOLVED that the WASHTO Policy Committee hereby adopts this Guide for Uniform Laws and Regulations Governing Truck Size and Weight among the WASHTO states; and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee supports the continuation of the work of the Subcommittee on Highway Transport to resolve highway transportation issues in the WASHTO states and to propose amendments to this guide; and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee encourages the individual state to pursue the necessary actions with various legislatures and policy making bodies to incorporate, to the extent possible, the recommendations of this Guide into the laws, regulations and policies of all of the WASHTO states in order to accomplish the desired uniformity of truck size and weight.

Adopted 4/6/90

RESOLUTION 7-93

RESOLUTION OF THE WESTERN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS

REGARDING ADOPTION OF THE SECOND EDITION OF "A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT AMONG THE WASHTO STATES"

WHEREAS, Resolution 90-1, approved by the WASHTO Policy Committee April 6, 1990, adopted the first edition of the "Guide;" and

WHEREAS, Resolution 90-1 recognized that the "Guide" should be a continuing and dynamic document; and

WHEREAS, the WASHTO Subcommittee on Highway Transport over the past three years has identified several needed modifications to the "Guide" and, accordingly, is proposing a second edition;

NOW, THEREFORE, BE IT RESOLVED that the WASHTO Policy Committee hereby adopts this second edition of "A Guide for Uniform Laws and Regulations Governing Truck Size and Weight Among the WASHTO State" (changes summarized in Attachment A); and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to support the work of the Subcommittee on Highway Transport to resolve highway transportation issues in the WASHTO states and to propose future amendments to the "Guide" as deemed necessary; and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to encourage the individual states to incorporate, to the extent possible, the recommendations of this "Guide" into the laws, regulations, and policies of all the WASHTO states in order to accomplish the desired uniformity of truck size and weight. The Policy Committee adopted this resolution at the WASHTO Annual Meeting in Oklahoma City, Oklahoma, on June 26, 1993.

RESOLUTION 7-95

RESOLUTION OF THE WESTERN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS

REGARDING ADOPTION OF THE THIRD EDITION OF "A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE & WEIGHT AMONG THE WASHTO STATES"

WHEREAS, Resolution 1-90, approved by the WASHTO Policy Committee, April 6, 1990, adopted the first edition of the "Guide"; and

WHEREAS, Resolution 1-90 recognized that the "Guide" should be a continuing and dynamic document; and

WHEREAS, Resolution 7-93, approved by the WASHTO Policy Committee, adopted the second edition of the "Guide"; and

WHEREAS, the WASHTO Committee on Highway Transport over the past two years has identified several needed modifications and additions to the "Guide" and, accordingly, is proposing a third edition;

NOW, THEREFORE, BE IT RESOLVED that the WASHTO Policy Committee hereby adopts this third edition of "A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE & WEIGHT AMONG THE WASHTO STATES" (Changes summarized in Attachment A); and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to support the work of the Committee on Highway Transport to resolve highway transportation issues in the WASHTO states and to propose further amendments to the "Guide" as deemed necessary; and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to encourage the individual states to incorporate, to the extent possible, the recommendations of the "Guide" into the laws, regulations and policies of all the WASHTO states in order to accomplish increased efficiency of interstate truck transportation in the WASHTO region.

Adopted by the WASHTO Policy Committee on July 17, 1995 at the WASHTO annual meeting in San Diego, California.

RESOLUTION 3-97

RESOLUTION OF THE WESTERN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS

REGARDING ADOPTION OF THE FOURTH EDITION OF "A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT AMONG THE WASHTO STATES"

WHEARAS, Resolution 1-90, approved by the WASHTO Policy Committee, April 6, 1990, adopted the first edition of the "GUIDE"; and

WHEARAS, Resolution 1-90, recognized that the "GUIDE" should be a continuing and dynamic document; and

WHEREAS, Resolution 7-93 and 7-95, approved by the WASHTO Policy Committee, adopted the second and third editions of the "GUIDE"; and

WHEREAS, the WASHTO Committee on Highway Transport over the past two years has identified several needed modifications and additions to the "GUIDE" and, accordingly, are proposing a fourth edition;

NOW, THEREFORE, BE IT RESOLVED the WASHTO Policy Committee hereby adopts this fourth edition of "A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT AMONG THE WASHTO STATES" (Changes summarized in Attachment A); and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to support the work of the Committee on Highway Transport to resolve highway transportation issues in the WASHTO member states and to propose further amendments to the "GUIDE" as deemed necessary; and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to encourage the individual states to incorporate, to the extent possible, the recommendations of the "GUIDE" into the laws, regulations and policies of all the WASHTO states in order to accomplish increased efficiency of interstate truck transportation in the WASHTO region.

Adopted by the WASHTO Policy Committee on July 30, 1997 at the WASHTO annual meeting in Sun Valley, Idaho.

RESOLUTION ____-99

RESOLUTION OF THE WESTERN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS

REGARDING

ADOPTING THE FIFTH EDITION OF "A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT AMONG THE WASHTO STATES"

WHEREAS, Resolution 1-90, approved by the WASHTO Policy Committee, April 6, 1990, adopted the first edition of the "GUIDE"; and

WHEREAS, Resolution 1-90 recognized that the "GUIDE" should be a continuing and dynamic document; and

WHEREAS, Resolution 7-93, 7-95 and 3-97, approved by the WASHTO Policy Committee, adopted the second, third and fourth editions to the "GUIDE" and, accordingly, is proposing a fifth edition;

NOW, THEREFORE, BE IT RESOLVED that the WASHTO Policy Committee hereby adopts this fifth edition of "A GUIDE FOR UNIFORM LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT AMONG THE WASHTO STATES" (Changes summarized in Attachment A); and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to support the work of the Committee on Highway Transport to resolve highway transportation issues in the WASHTO member states and to propose further amendments to the "GUIDE" as deemed necessary; and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee continues to encourage the individual states to incorporate, to the extent possible, the recommendations of the "GUIDE" into laws, regulations and policies of all the WASHTO states in order to accomplish increased efficiency of interstate truck transportation in the WASHTO region.

Adopted by the WASHTO Policy Committee in January 2000 by mail ballot.